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College of Humanities, Language Studies, Journalism and Communications
School of Journalism and Communications

Assessment of Awareness Raising Efforts by Addis Ababa City Administration Traffic Management Agency to Reduce Road Traffic Crashes

By: Elias Tesfaye

Advisor: Dr. Solomon Ghebre-Ghiorghis

A Thesis Submitted to the Addis Ababa University School of Graduate Studies School of Journalism and Communication in Partial Fulfillment of the Requirements for the Degree of Master of Arts in Public Relations and Strategic Communication

Addis Ababa

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DECLARATION

I, the undersigned, hereby declare that this thesis entitled “*Assessment of Awareness Raising Efforts by Addis Ababa City Administration Traffic Management Agency to Reduce Road Traffic Crashes*” is the bona fide work of my original research and has not been presented for a degree in any other university or institution, at least to my knowledge. All the sources of materials used for the study have been duly acknowledged.

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This is to certify that the thesis prepared by Elias Tesfaye, entitled *Assessment of Awareness Raising Efforts by Addis Ababa City Administration Traffic Management Agency to Reduce Road Traffic Crashes*. And submitted in partial fulfillment of the requirements for the Degree of Master of Arts in Journalism and Communication complies with the regulations of the University and meets the accepted standards with respect to originality and quality. The work is eligible to earn master's degree in journalism and communication.

Signed by Examining committee

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Examiner _____ Signature _____ Date _____

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Chair of Department or Graduate Program Coordinator

ABSTRACT

Road traffic crash has been a horrific and devastating life situation every single day. In Addis Ababa on average one person loses his/her life per day or 480 people die per year on the roads. The research is conducted to assess the awareness raising efforts by Addis Ababa City Administration Traffic Management Agency (AATMA) to reduce road traffic crashes. To meet the objective of the study, both quantitative and qualitative methods of data collection were used. A questionnaire was administered to 141 respondents. In addition, in- depth interviews and a focus group discussion session were held with participants from AATMA. Qualitative data was presented by descriptive narration and the quantitative one was analyzed using SPSS version 20 and presented in the form of table and chart. The findings of the study showed that the road safety awareness raising efforts of the Agency include among other things offering awareness-raising education mostly in a planned and organized way, preparing contents that address the behavioral and ethical problems of road users, working with education sector to include road safety and traffic management system in the curriculum, working at grass roots level i.e. having specialized and scheduled awareness creation plan at public gatherings, utilizing media to educate or inform the public despite some constraints. Hence, the findings of the study revealed that the awareness-raising efforts played key role for the reduction of road traffic crashes. The study shows that such decline is due to a combined effort of the three interventions of the Agency: education, enforcement and engineering though the Agency gives first priority to engineering. Based on the findings of the study, the researcher suggests possible recommendations for the problem. These include media are believed to be key strategy and upper hand of awareness-raising strategies. The finding of this study revealed that due to budget constraints and harsh financial rules, the Agency is not fully utilizing the electronic media. To conduct up-to-date and better road safety awareness-raising programs using the media, the City Administration has to allocate sufficient budget for awareness-raising programs. The upper management of the Agency has to remain engaged in lobbying media houses and other concerned bodies to largely discount road safety program costs as road safety issue is a common social responsibility. Eventually, the study gives some suggestions for further research.

TABLE OF CONTENTS

<i>ABSTRACT</i>	iii
TABLE OF CONTENTS.....	iv
LIST OF TABLES.....	vii
LIST OF CHARTS.....	viii
ACRONYMS.....	ix
ACKNOWLEDGEMENTS.....	x
1.1. Background of the Study.....	1
1.2. Statement of the Problem.....	2
1.3. Objectives of the Study.....	3
1.3.1. General Objective.....	3
1.3.2. Specific Objectives.....	3
1.4. Basic Questions.....	4
1.5. Significance of the Study.....	4
1.6. Scope of the Study.....	4
1.7. Limitations of the Study.....	5
1.8. Structure of Thesis.....	5
CHAPTER 2: REVIEW OF THE RELATED LITERATURE.....	6
2.1. Awareness-raising.....	6
2.2. Importance of Awareness-raising.....	7
2.3. Ways of Road Safety Campaign.....	7
2.4. Communication Approaches to Awareness-raising.....	8
2.5. Road Safety Education.....	9
2.6. Media and Audience.....	10
2.7. Researching Audience.....	11
2.8. Media Influences on public.....	12
2.9. Road Traffic Crash and its Causes in Addis Ababa, Ethiopia.....	13
2.10. Intervention Strategies to Reduce Road Traffic Crashes.....	14
2.11. Road Safety Interventions in Ethiopia.....	15
2.12. Road Safety Interventions in Addis Ababa.....	16
2.13. Behavioural Change Communication.....	17
CHAPTER 3: RESEARCH METHODOLOGY AND DATA ANALYSIS PROCEDURES	19
3.1. Demographics of Addis Ababa.....	19

3.2. Research Design.....	19
3.3. Data Sources	20
3.4. Instruments of Data Collection	20
3.4.1. Questionnaire	21
3.4.2. Individual in-depth Interview.....	21
3.4.3. Focus Group Discussion (FGD).....	21
3.5 Data Gathering Procedure and Piloting Instruments.....	22
3.6. Methods of Data Analysis and Presentation	22
3.7. Methods of Samplings	23
3.8. Research Site.....	24
3.9. Ethical Considerations	24
CHAPTER 4: RESULT AND DISCUSSION	25
4.1. Presentation of Quantitative Results.....	25
4.1.1. Employees’ perception towards awareness-raising activities performed by the Agency, strong points as well as weaknesses noticed.....	25
4.1.2. Employees’ judgment about Agency’s media utilization performance to raise public awareness.....	30
4.1.3. The cooperation of stakeholders in the Agency’s awareness-raising activities	32
4.2. Description of Qualitative Results	42
CHAPTER 5: CONCLUSIONS AND RECOMMENDATIONS	47
5.1.Conclusions.....	47
5.2.Recommendations.....	48
5.3. Suggestions for Further Research	49
REFERENCES	50
APPENDIX A.....	53
APPENDIX B	55
APPENDIX C	57
APPENDIX D.....	59
APPENDIX E	60
APPENDIX F.....	62
APPENDIX G.....	63
APPENDIX H.....	64
APPENDIX I	65
APPENDIX J	66

APPENDIX K.....	67
Director of Road Safety Education and Capacity Building Directorate, Genet Dibaba.....	69

LIST OF TABLES

Table 1: Analysis of variables related to background of respondents in AATMA

Table 2: Employees' perception towards awareness-raising activities performed by the Agency

Table3: Employees' perception towards Agency's media utilization performance

Table 4: Attitude of employees towards stakeholders' participation in Agency's awareness-raising programs

Table 5: Analysis of variables related to background of respondents in AATP

Table 6: Background of Pedestrian Respondents

Table 7: View and action of Addis Ababa Pedestrians to the questions related to awareness-raising activities of AATMA

LIST OF CHARTS

Chart 1: Reaction of members of AATP to awareness-raising activities of the Agency and its relationship with AATP

Chart 2 : Agency Cooperation with AATP

Chart 3 : Outlook of members of AATP about Agency communication methods

ACRONYMS

List of Acronyms

AATMA - Addis Ababa Traffic Management Agency

AATP- Addis Ababa Traffic Police

FGD- Focus Group Discussion

GNP - Gross National Product

GTP- Growth and Transformation Plan

RSE- Road Safety Education

RTA-Road Traffic Accidents

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CHAPTER 1: INTRODUCTION

1.1. Background of the Study

Road traffic crash has been a major impediment of social and economic well-being of all nations of the world. Each day it is abruptly claiming the lives of thousands of people. Facts and figures indicate that the world is losing more than a million people per year due to road traffic crashes. Despite being entirely preventable, road traffic crash is causing injuries to significantly high number of people around the globe. (Addis Ababa Annual Road Safety Report (2018-2019))

Although it is given less attention and effort, road traffic injury has been a matter of global health problem and taking the lives of productive citizens. Addis Ababa Annual Road Safety Report (2018-2019, P. 2) states road traffic injuries are a major but neglected global public health problem, requiring concerted efforts for effective and sustainable prevention. Goulden (2020, P.1) adds there are around 1.35 million road traffic deaths globally each year, making it the 8th leading cause of death and the number one killer of people aged 5 to 29.

Similarly, Awtachew et al. (2020, P.1) further describe road traffic injury is now the leading cause of death for children and young adults aged 5-29 years, signaling a need for a shift in the current child health agenda. It is the eighth leading cause of death for all age groups exceeding HIV/AIDS, tuberculosis and diarrheal diseases, and the deaths due to road traffic accidents (RTA) were predicted to become the fifth-leading cause of death by the year 2020.

There are a number of contributing factors causing road traffic crashes. The causes range from infrastructure development to human behavior and skill. Awtachew et al. (2020, PP. 1-2) found that although road infrastructures have a significant role in the occurrence of RTA, the human factor is the most prevalent contributing factor of RTAs. This includes both driving behavior (e.g., drinking and driving, speeding, traffic law violations) and impaired skills (e.g., lack of attention, exhaustion, physical disabilities and so on).

Reports show that Ethiopia is among highly vulnerable countries to road traffic crash. World Health Organization report (2011) mentioned in Awtachew et al. (2020, P.2) explains RTA is the ninth killer health problem in the country. RTA makes Ethiopia 12th and 9th in the world and in Africa respectively.

Addis Ababa is not only the capital of Ethiopia but the seat of African Union, embassies and other international organizations. According to statistical profiles, the City has a population size of more than four million. It is the centre of the economy and the home of people with diverse social and economic background. More than half of the vehicles in Ethiopia are found in Addis Ababa (Addis Ababa Annual Road Safety Report 2018-2019, P. 3).

As to the report of Addis Ababa Annual Road Safety Report (2018-2019, P.2) 13 people lose their lives and 37 people are injured every single day due to road traffic crash in Ethiopia. Similarly, in Addis Ababa on average one person loses his/her life per day or 480 people die per year on the roads. Moreover, road traffic crash is among serious problems resulting in impacting social and economic life of citizens causing psychological and financial crises. Hence, road traffic crashes contribute to poverty, loss of productivity, material damage, injuries, disabilities, grief and deaths.

Addis Ababa Traffic Management Agency was established in the year 2015 with a mandate of ensuring safe and acceptable traffic movement in the City by deciding, informing, implementing and controlling proper road use rules. In order to meet its institutional responsibilities, the Agency must work and have capacity on the five crucial areas: data and knowledge, engineering and technology, enforcement, parking management, and awareness and education (Traffic Management Strategy).

1.2. Statement of the Problem

Transport and mobility are central to ensuring coexistence and interdependence among the people of the world and are profoundly essential for the development of civilizations. Transport implies the movement of people or vehicles from one location to another. The act of such movement is commonly termed as traffic. In the course of movement or traffic, however, the world has so far experienced not only impressive outcomes but devastating consequences. Millions of people die on the roads every year due to traffic crashes (Goulden, 2020).

Hence, Addis Ababa City Administration has prepared strategies and programs to ensure safe and acceptable traffic movement in the City. A study reveals that more than half of road traffic crashes in Ethiopia occurs in Addis Ababa. Most of the crashes are associated with human error (African Research Review, 2017). Gouldon (2020) substantiates this concept and adds that the causes of road traffic crashes like speeding, not wearing a seat belt, drink

driving are preventable and avoidable behaviours. And yet public awareness campaigns have the potential to bring about behavioural transformation to save lives.

So as to realize road traffic safety and reduce traffic crashes, therefore, education is a pillar program. Sarmad et al. (2019) describe road safety education is one of the five E's in order to increase road safety: Encouragement, Enforcement, Evaluation, Engineering and Education. It has been recognized as an instrument for reducing road traffic accidents.

Education is a means to bring behavioural change. Efforts being undertaken to reduce or bring zero traffic crash will be supported with awareness-raising programs. Hence, as a pillar strategy, Addis Ababa Traffic Management Agency (AATMA) is implementing awareness-raising programs to reduce road traffic crashes.

So far studies conducted on road safety issues in Ethiopia, Addis Ababa include: Effectiveness of an improved road safety policy in Ethiopia: an interrupted time series study by TeferiAbegazet. al and effectiveness of a drinking and driving campaign on knowledge, attitudes, and behavior among drivers by Singh, N.N. This study encompasses the whole range of awareness raising efforts by the Agency.

It is known that the Agency is a new organization established in 2015. This study is, therefore, the first one to be conducted on the assessment of awareness-raising efforts of the Agency in combating road traffic crashes. I hope it can be used as a baseline survey.

The main intention of this study is, therefore, to assess the awareness raising efforts being carried out by the Agency to reduce road traffic crashes.

1.3. Objectives of the Study

1.3.1. General Objective

The main objective of this study is to assess the awareness raising efforts by Addis Ababa City Administration Traffic Management Agency to reduce road traffic crashes.

1.3.2. Specific Objectives

The specific objectives of this research are to:

1. Show the contribution of the awareness-raising interventions in the reduction of road traffic crashes and the public view towards awareness-raising activities.

2. Identify the strong points and weaknesses as well as major challenges of awareness-raising activities of the Agency to reduce road traffic crashes.
3. Investigate the level of cooperation of stakeholders in the awareness-raising activities.
4. Assess the awareness-raising efforts of the Agency to reduce road traffic crashes.

1.4. Basic Questions

In this study, attempts will be made to answer the following questions:

- a. What activities does the Agency perform to raise public awareness about road safety and the contributions it brings?
- b. What are the strong points and the weak sides as well as the challenges of the awareness-raising activities of the Agency?
- c. What is the public view and action towards awareness-raising activities?

1.5. Significance of the Study

Doing road safety awareness-raising research is crucially important in the struggle against road traffic crashes. The researcher believes that this study would have some contribution to the existing and future strategic response to road safety issues. Apparently, policy makers, researchers, governmental and non-governmental organizations are among the possible beneficiaries of the study. Particularly, AATMA will benefit a lot from this study in identifying the gaps noticed so far in road safety awareness-raising programs and in the meantime reviewing its awareness-raising interventions.

1.6. Scope of the Study

Despite a few more intervention strategies of the Agency for reducing road traffic crashes, due to factors of time and expense this research was confined to education, awareness-raising activities being carried out by Road Safety Education and Capacity Building Directorate of Addis Ababa City Administration Traffic Management Agency. The Directorate is entitled to carry out road safety education and conduct capacity building activities to reduce road traffic crashes. The Agency operates only in Addis Ababa and so is this study confined to activities carried by the Directorate of the Agency. According to Addis Ababa Annual Road Safety Report (2018-2019, P.3) more than half of the country's vehicles are found in Addis Ababa where highest traffic is noticed.

1.7. Limitations of the Study

During the study, constraint of finance and time limited the researcher to increase the sample size of the study. Thus, data from pedestrians were gathered only from two sub cities: Yeka and Bole.

1.8. Structure of Thesis

The research is organized into five chapters. Chapter One covers introduction, statement of the problem, research questions, research objectives, significance of the study, scope of the study, limitations of the study and structure of thesis. Chapter Two is all about review of the related literature. Research methodology and data analysis procedures are entertained in Chapter Three. Chapter Four is about research and discussion of the paper. Finally, the conclusion, recommendations of the study and suggestions for further research are included in the last Chapter of the Paper.

CHAPTER 2: REVIEW OF THE RELATED LITERATURE

2.1. Awareness-raising

Awareness-raising or awareness creation plays key role to bring change in outlook or action. It is one of the road safety actions that help change or influence the attitudes and behaviors of a target group, in this case actors in road transport i.e., road users that include drivers, passengers and pedestrians.

Awareness-raising is a broad and somewhat vague term, yet one that is almost intuitively understood in most societies and cultures. To raise awareness of something – good, bad or indifferent- is to promote its visibility and credibility within a community or society. To raise awareness is also to inform and educate people about a topic or issue with the intention of influencing their attitudes, behaviors and beliefs towards the achievement of a defined purpose or goal. The awareness of people must also be raised in order to ensure that correct actions can be taken so the likelihood of occurrence of bad or devastating situation can be avoidable.(Sayers 2006, PP. 10-11)

According to Cardinal, N. (n.d, P.1) “Awareness-raising is a process that seeks to inform and educate people about a topic or issue with the intention of influencing their attitudes, behaviours and beliefs towards the achievement of a defined purpose or goal. It can mobilize the power of public opinion in support of an issue and thereby influence the political will of decision makers.” Therefore, we can deduce that awareness-raising activities can shape the dynamic nature of human behavior to a desired goal in due course become politically influential to have a favourable decision.

Concepts such as ‘awareness-raising’ are so often used in different circumstances that they can sometimes appear to the layman as a technocratic jargon. Awareness-raising means making a group of people aware of something. Associated with the word “campaign” the concept of “awareness-raising” refers in concrete terms to a communication, a promotion or an information campaign. The European Solar Thermal Industry Federation et al (2012, P.1)

Awareness-raising can be grouped under road safety education. The five E’s to manage road safety include Education, Enforcement, Engineering, Environment and Emergency service. (Maqbool et al. 2009, P.98)

Drivers' behavior can be improved through safety educational programs. Education and awareness-raising are used to provide the information regarding road safety and different media are used by the host country. The government plays an active role in conducting awareness among road users by improving road safety through campaigns and educational programs. Road safety education and awareness for children and rural people is an effective tool for improving the behavior of road users. Education provides the good senses to all the road users resulting into improved driver behavior. (ibid., 2011)

Therefore, education/ awareness-raising is one of the key strategies to manage road safety programs. Any road safety management program devoid of education/ awareness-raising will be futile and ineffective. Education/ awareness-raising programs not only changes road users' behavior but has the potential to influence government policies and programs to gear towards minimizing or avoiding road traffic crashes.

2.2. Importance of Awareness-raising

Cardinal (n.d, P.1) adds that awareness-raising and dissemination of information is critical to create an enabling environment for accountability, promote participatory and inclusive processes for follow up and review, and build ownership. Awareness-raising programs can, therefore, be used to promote an understanding and practice of existing traffic laws and the rights to use roads.

Raising awareness is necessary to make governments and stakeholders responsible and accountable for road safety. Cardinal (n.d) states that raising public awareness is also essential to bring about necessary changes to achieve ambitious changes.

According to Arthritis National Research Foundation (2020) awareness-raising matters because it can help to improve the lives of people. Knowledge is a powerful tool that causes people to take action that can improve an existing situation. Increased understanding about existing conditions in the community can have some powerful effects. It can encourage others to support funding of research or passing of legislation that will help the cause.

2.3. Ways of Road Safety Campaign

In conducting road safety awareness-raising programs, we can grasp the attention of people by using different ways of educating people because not all people have the same understanding level. On top of this, only one way of road safety awareness-raising campaign can be monotonous. Therefore, it is more preferable to present different ways of conducting

public awareness-raising campaign. Goulden (2020, PP. 1-2) mentions, “Effective road safety campaign can inspire change using the following smart ways:

1. **Making it emotive:** emotion is a powerful tool in any project looking to inspire behavioural change. For instance, persuading drivers not to text while driving.
2. **Using behavioural insights:** draw on behavioural science to reduce reckless behavior among young male drivers, the group most at risk of death in road accidents. This relies on assumption that people drive differently depending on who is in the car.
3. **Using powerful imagery:** this is the act of using billboards to represent the devastating impacts of a crash.
4. **Choosing audience carefully:** focus a campaign on the families and friends of drivers, rather than the drivers themselves.
5. **Drawing on scientific evidence:** using scientific research on road safety to demonstrate risks and convince your audience to prioritize safety.”

2.4. Communication Approaches to Awareness-raising

Awareness-raising is not an easy task requiring multifaceted efforts and approaches to be effective and succeed in meeting its underlying goals. Sayers explained (2006, PP. 43-58), “There are different types of awareness-raising approaches. The more commonly used approaches maybe grouped into five broad categories with each describing its primary approach or emphasis:

***Personal communication:** a personal approach is often the most effective means of selling a good idea, particularly if the approach comes from, or is sponsored by, an individual or organization with recognized credibility.*

***Mass communication:** while personal communication tends to be the most effective means of raising awareness of issues in smaller communities, it is not always the most efficient strategy for communicating a message widely. To achieve this, we must rely on mass communication through the ‘mass communication’. Examples of mass communication include: printed materials, audiovisual resources, websites, media interviews, news items on local radio and television and broadcast SMS messages to mobile telephones.*

From the above explanations the researcher could succinctly deduce that the two approaches are situational dependent, complementary and comparable.

Education: *an increasingly important educational approach involves promoting and teaching information literacy skills within communities, often starting with children in schools. Two types of education are relevant to awareness-raising campaigns: formal education and informal education.*

Public Relations: *the planned and sustained effort to establish and maintain goodwill and mutual understanding between an organization (awareness-raising campaign) and its publics (audience and stakeholders)*

Advocacy and lobbying *efforts are sometimes overlooked when planning awareness-raising campaigns but can be vital to ensuring ongoing support from governments and civil society organizations.”*

Sayers (2006, P.46) remarks, “Depending on the circumstances of the campaign, an effective communications strategy should ideally incorporate a mix of one or two approaches from at least three of the five broad categories.”

It is further explained, “Every awareness-raising campaign requires some measure of public involvement to be effective.” (ibid., 2006). From the above explanations, therefore, we can remark that awareness-raising is carried out using various methods depending on the existing situations.

2.5. Road Safety Education

Road Safety is defined as a key pillar of traffic safety work. Road Safety Education (RSE) summarizes the totality of measures that aim at positively influencing traffic behavior patterns. RSE emphasizes on promotion of knowledge and understanding of traffic rules and situations, improvement of skills through training and experience, strengthening and/or changing attitudes towards risk awareness, personal safety and the safety of other road users. (European Commission, 2005, P.5)

There has been comparison and speculation as to which aspect of road safety is effective and prioritization is given (enforcement, engineering or RSE). Historically, there is no clear evidence that RSE has been effective in reducing rates of road traffic crashes, whereas such evidence exists for enforcement or engineering measures. Thus, RSE often suffers from lack

of prioritization in traffic safety work in general, and specifically in educational work in schools. However, one has to keep in mind that effective RSE includes a key element which none of the other road safety key interventions, like engineering, enforcement or licensing training covers. This key element is creating and changing attitudes towards safe and socially responsible behavior in traffic. Besides, the long-term perspective of developing attitudes, RSE must also contribute to an important immediate goal. One has to be aware that traffic accidents are the main cause of fatality for teenagers, ranking before disease. (ibid)

2.6. Media and Audience

We have got two types of media: print and electronic whose prime purpose is to inform and entertain their audience in an organized way. The ultimate objective of media, therefore, is to offer information and entertain audience; conversely the audience or public access to information depends on media. According to Solomon (2000, P.34) media and audiences have gone through various historical stages through which they could evolve overtime. In ancient times, media audiences as a set of spectators for public events were institutionalized and localized. Currently radio listeners, TV and cinema watchers, etc. comprise media audience.

As cited by Olasunkanmi (2017, P.3) Denis Mc Quail (2000) states that audience has long been familiar as the collective term for the receivers in the simple sequential model of the mass communication process (source, channel, message, receiver, effect) that was deployed by pioneers in the field of media research. He suggests that conceptualizing the audience within the linear model can be problematic due to audience fluctuating abstract characters.

Media scholars have come to hold the opinion that the concept of ‘audience’ can change overtime this is due to the direct or indirect changes and multiplication of communication channels on what is traditionally understood as audience.

Mc Quail (2010, P.447) writes,

“Sufficient reasons have been given to wonder whether the term ‘audience’ is still a useful one, especially as there are so many kinds of use of many different media. The term ‘audience’ cannot easily be divested of its strong connotation of ‘spectatorship’- of rather passive watching and listening.”

From Mc Quail point of view, Olasunkanmi (2017, P.4) commented that audience as a concept cannot be denied or disassociated from its originating meaning despite the facts that emerging media platform are eroding and reshaping ‘audience’ conformist sense.

By conformist sense, we mean the traditional explanation by pioneer scholars in the field of media and communication research.

Awareness-raising efforts require insightful categorization of the community so that appropriate and relevant messages reach the audience. Sayers (2006, P.67) further explained,

“When stakeholders are planning awareness-raising campaigns, it is absolutely critical that audiences with special needs are not overlooked. Audiences that might typically require special consideration include: aged members of the community, those with infirmities and disabilities, people challenged by learning difficulties, illiteracy or limited access to education, adult learners and children.”

Therefore, we can say that prior to conducting awareness-raising programs, it is important to consider the various grouping of the community we want to communicate or target so that we can achieve our objectives.

“Raising-awareness within special audiences need not be an onerous or expensive undertaking as simple and cost-effective solutions are often available. For example, communicating a message to people with blindness or vision impairments might be as simple as ensuring that the awareness-raising strategy includes radio broadcasts, so the message can be heard rather than read. Alternatively, printed materials can be reproduced in Braille or recorded onto tapes or compact discs for later playback. Similarly, those with hearing problems will benefit from pamphlets, posters and the simple inclusion of a signing interpreter at public meetings and forums, and in television broadcasts.” (ibid.,67)

To sum up, therefore, awareness-raising tasks should be affordable and not budget intensive if handled and managed in compliance with audience need and background.

2.7. Researching Audience

We can investigate media from different perspective using different tools and groups such as counting viewers for advertising purpose. Focus groups are employed to see how the target demographic is receiving a particular text. Solomon (2000, P.35) explains, “Three

approaches to media audience research which are comparable to those used in the study of media structures and institutions. These are in the structural, behavioural and social-cultural traditions:

***Structural Approach:** the goal of such research is essentially descriptive of the audience in terms of its composition and its relation to the social structure of the population as a whole. It is carried out on behalf of media organizations.*

***The Behavioural Approach:** originated in the field of social psychology and sought to establish the effects of media messages on individual behavior, opinions, attitudes and values.*

***The Social- Cultural Approach:** the most recent addition to the range of audience research approaches derives mainly from literary criticism and cultural studies.*

2.8. Media Influences on public

According to Solomon (2000, P.38) the media have credible power to shape opinion and belief, to change habits of life and to mould behavior actively. Such views, however, were not based on scientific investigation but on observation of the enormous popularity of the press and of the news media of film and radio which intruded into many aspects of everyday life as well as public affairs.

Therefore, the media are vitally important tools or weapons through which the psychological and social make up of a public can be shaped or altered. Through media, we can conquer and control the minds of people and win a battle field- any social, political or economic issue. So, media can be as powerful as a state.

Likewise, thanks to technological advancement, the expansion of local or international outreach of information through media has been rife resulting in the building up of multicultural and global society. Certainly, the attitude and behaviour of people are being affected more and more by the increasingly interconnected and interdependent activities of media houses across the globe (ibid., 2000). Therefore, we can assert that media have profound influence on interactions and actions of domestic or international publics.

However, the point of departure comes when we consider and assess the fact that what segment of the world population is using the massive and credible advancement in communication technology and information explosion to matters of national and international concern. Solomon (2000, P.39) argues in the world where information explosion is experienced especially with the introduction of electronically based new technologies in the press, radio and television and more recently the internet, e-mail, cable and satellite communication, etc., it is thought that people would access overflow of information in their personal lives. However, research seems to suggest the failure of large segments of the population to share in the information explosion, with the revelation of shocking public ignorance on matters of basic national and international concern. This is partly blamed on the “ineffectiveness” of the media.

2.9. Road Traffic Crash and its Causes in Addis Ababa, Ethiopia

More than half of road crashes in Ethiopia occurs in Addis Ababa. Most of the crashes were due to the driver’s not yielding right of way to other vehicles. Generally, among other risk factors we assessed including road conditions and asphalt pavement, drivers not yielding right of way to pedestrians and not yielding right of way to other vehicles were the leading risk factors for road crashes in the city (African Research Review, 2017). From this we can deduce that road crashes in Addis Ababa is caused by or associated with human error or behavior.

Studies show that road crashes have had opposite trends in developed and developing countries decreasing and increasing trend respectively. African Research Review (2017) describes whereas international studies have shown that road crashes in developed countries have been decreasing in the past decades, in developing countries the opposite trend has become apparent. Considering the trend of road crashes in Addis Ababa, Ethiopia, completely the opposite is apparent. Studies revealed that Ethiopia is reported to be the country with deadliest roads in the world, indicating a road crash fatality rate of at least 114 per 10, 000 vehicles per year, compared to only 10 in the UK and Ireland and 60 across 39 sub-Saharan African countries. Furthermore, the number of people injured or killed in one crash in Ethiopia is about 30 times higher than in the US. Addis Ababa- Ethiopia’s capital city shares 60% out of the total number of vehicles in the country while its population is only about 3.2 % of the total population for the country.

Such unacceptably high road traffic crashes statistics in Addis Ababa are attributed to complex interrelated risk factors ranging from driver and pedestrian behavior, traffic rules and regulations, and vehicle and road conditions. For instance, the Ministry of Infrastructure, Road Authority (2005) report showed that problems related to the road itself were only responsible for about 1 to 3 % while the largest crashes were attributed to driver error, which accounted for 81%. Similarly, Tesema et.al. (2005) in their study of road traffic crashes in Ethiopia, reported that about 81% of the road traffic crashes countrywide are due to driver errors such as: drivers not respecting pedestrian priority, speeding, unsafe utilization of freight vehicles for passenger transportation, poor driving skills and undisciplined driver behavior. Therefore, the very cause of road traffic crashes can be preventable through awareness-raising and training as it is a human factor.

African Research Review (2017) further explains other attributing factors contributing to road crashes: a lack of advanced technology for controlling road crashes in Addis Ababa, a disproportionately low number of traffic police officers per number of vehicles and roads, bad crossing behaviors of pedestrians, ineffective or nominal road traffic laws and regulations were risk factors of road traffic crashes in Addis Ababa, Ethiopia.

According to Addis Ababa Annual Road Safety Report (2018-2019, PP.7-8), after continuous increases in road traffic deaths in Addis Ababa, data of the past three years show the number has stabilized, with virtually no change since 2016-2017. It was highlighted in the report that according to the 2018-2019 federal police commission statistics report, the number of road traffic deaths and injuries in Ethiopia dropped by 10% and 13% respectively in the past year.(For more information see appendix K)

2.10. Intervention Strategies to Reduce Road Traffic Crashes

According to Bonnet et.al (2018, P.1) the highest number of deaths due to road traffic crash occurs on the African continent at the rate of 26.6 deaths per 100,000 inhabitants among whom young Africans are the most victims which results in taking away 1% of Gross National Product (GNP) in low-income countries. Hence, following a sharp increase in motorization in the continent, actions must be taken to reduce road crash and protect populations. So far, an overall framework for road safety activities has been designed and proved their potential effectiveness in low and middle income countries. For instance, helmet use in Cambodia has been effective but the same activities have not been practiced in every country i.e., few interventions have been put in action on African continent. As a result, the

number of deaths due to road traffic crash remained high and no significant improvement has been observed.

Five intervention pillars across the entire spectrum of road traffic accidents and their consequences have been practiced. The interventions were unevenly distributed according to their fields. These types of intervention provided information about the implementation and the panel explored in an attempt to reduce the burden of road traffic accidents. (Bonnet et.al, 2018, PP. 9-14)

The types of intervention include: media, training, technologies and data bases, safety policy and program, and highway equipment and improvements

- i. **Media:** to warn and raise awareness in connection with a given problem.
- ii. **Training:** to conduct training sessions to learn about road safety signage.
- iii. **Technologies and databases:** to analyze road traffic accidents and improve road safety.
- iv. **Safety policy and program:** dealt mainly with public policies in which laws were passed and the impact analyzed.
- v. **Highway equipment and improvements:** dealt with the distribution and installation of highway equipment with the purpose of lowering traffic speed or protecting pedestrians. (ibid, 2018)

Conversely, the developed world has reached the bottom level a sharp decline in road traffic crash using a different intervention strategy. Evidence from high income countries showed that implementation of appropriately designed and well enforced road safety policy contribute a lion share to their impressive achievements of the declining trend in the number and severity of crash.(Abegaz et.al, 2014, P.1)

2.11. Road Safety Interventions in Ethiopia

Road traffic crash has remained a global health crisis due to which more than a million people lose their life. It has had devastating consequences especially in the developing world. Abegaz et al. (2014, P.1) describe that road traffic crash is a growing public health threat, being responsible for 1.2 million deaths and up to 50 million non-fatal injuries globally. It is a big challenge especially for low- and middle-income countries, 90% of the victims were found. Accident trend observed in industrial countries witnessed the rapid reduction over the last three to four decades, while a terrifying increment reported from a number of developing

countries including Ethiopia. The problem may grow further in the coming decades; due to the rapid rising of vehicle ownership associated with their economic growth.

Developing countries are taking serious actions to avert deaths and injuries due to road traffic crash as a result encouraging results have been registered. In recent years, many low-income countries are implementing improved road safety policy to deter risky driving practices including- exceeding the speed limit, impaired driving by alcohol and drugs, phoning and texting while driving, and not using seat belt. (ibid.)

There are different governmental and non-governmental actors operating in road safety programs in Ethiopia to address road traffic crashes. Addis Ababa Road Traffic Management Agency (2016) highlights Federal Transport Authority is one of the key players which are striving to address the problem of car crash and its excruciating consequences. Accordingly, one of its strategies that the Authority employs to address the problem is conducting researches on various issues and sharing them with its partners. Ethiopia is one of the countries which are suffering from severe car crash problem. In order to address the problem, the country has designed a 10-year action plan i.e., 2003- 2012 Ethiopian calendar. The Growth and Transformation Plan 2 (GTP 2) rate which demonstrates 65 deaths from among 10 thousand vehicles in one year is envisaged to reduce to 27 by 2012.

2.12. Road Safety Interventions in Addis Ababa

Addis Ababa Annual Road Safety Report (2018-2019, P.2) explains there is no single day free from a serious road traffic crash with approximately one death in the City. The City Administration of Addis Ababa is committed to taking measures to reduce road traffic crashes and ensuring safe and acceptable traffic movement in the City. The measures can range from establishing accountable institutions to developing a strategy. Traffic Management Strategy (2019, P.1) explains the city administration is taking measures to solve the transport problems at large and traffic challenges in particular. To create the capacity of handling the problem, the Addis Ababa City Transport Bureau was restructured and new institutions were established under the Bureau. Among the new institutions, Addis Ababa Traffic Management Agency is one.

The City Administration has taken various road safety actions to reduce road traffic crashes. The actions include among other things media campaign on drink driving, on speeding as well as on enforcement and conducting workshop on data management and operational

planning. The Report states due to a significant percentage drop in drink driving as a result of media campaigns supported by enhanced enforcement, the city administration implemented another media campaign on drink driving. The media campaign and enforcement activities were also supported by earned media activities. (Addis Ababa Annual Road Safety Report, 2018-2019, P.32)

2.13. Behavioural Change Communication

As to Center for Global Health Communication and Marketing behavior change communication (BCC) is the strategic use of communication to promote positive health outcomes, based on proven theories and models of behavior change. BCC employs a systematic process beginning with formative research and behavioral analysis, followed by communication planning, implementation, and monitoring and evaluation. Audiences are carefully segmented, messages and materials are pre-tested, and both mass media and interpersonal channels are used to achieve defined behavioral objectives

Behaviour is a complex phenomenon, influenced by factors within the individual and beyond. The Social Ecological Model recognizes four levels of influence that interact to affect behavior: individual, family and peer networks, community and social. (Johns Hopkins University, 2020)

According to Johns Hopkins University (2020) previously known as behavior change communication (BCC), SBCC is the strategic use of communication approaches to promote changes in knowledge, attitudes, norms, beliefs and behavior. The terms BCC and SBCC are interchangeable, and they both refer to the coordination of messages and activities across a variety of channels to reach multiple levels of society, including the individual, the community, services and policy. SBCC is grounded in theory and is evidence-based.

Behavioral Change Communication has its own operational settings and roles. Nancy and Amol describe this as,

“Behavior change communication (BCC) plays a key role in clinical settings and public health in the prevention of both communicable and non-communicable diseases. In the future, it aids in tackling the outbreaks from the emerging and re-emerging infectious diseases. BCC is “an interactive process with communities (as integrated with an overall program) to develop tailored messages and approaches using a variety of

communication channels to develop positive behaviours; promote and sustain individual, community and societal behavior change; and maintain appropriate behaviours. A BCC strategy in any program should have a theoretical base for effective planning and evaluating an intervention. This review article critically appraises the evolution of BCC, the strengths and weaknesses of BCC theories/models and its applicability from the past to the future. This review will benefit postgraduates and public health workers in understanding the concepts of BCC and applying the same in their practice.”

CHAPTER 3: RESEARCH METHODOLOGY AND DATA ANALYSIS PROCEDURES

This chapter presents demographics of Addis Ababa, methods of the study, data sources, instruments of data collection, data gathering procedure and piloting instruments, methods of data analysis and presentation, methods of sampling, research site, and ethical considerations applied in the research.

3.1. Demographics of Addis Ababa

Addis Ababa is the largest city in the world located in a land locked country, holding 527 square kilometers of area in Ethiopia. The population density of the city is estimated to be about 5, 165 individuals per square kilometer available. In the near future the city is expected to be a home of 6.5 million of residents (World Population Prospects, 2019 Revision, P. 1)

Addis Ababa is the educational and administrative centre of Ethiopia. The bulk of the export and import trade of Ethiopia is channeled through Addis Ababa on its way to and from the port of Djibouti (Augustyn, A. 2021)

Being the central gravity of foreign and inland trade of the country, Addis Ababa is experiencing rapid expansion and traffic flow which lead to severe traffic congestions and crashes. Facts and figures show that the City is having high population and vehicle growth from year to year. This in turn could be one of contributing factors to higher road traffic crashes. Thus, as stated earlier on average the life of one person is lost per day or 480 people die per year on the roads. (Addis Ababa Road Traffic Management Agency, 2016)

3.2. Research Design

The research design is the overall strategy that the researcher chooses to integrate different components of the study in a coherent and logical way, thereby, ensuring it effectively addresses the research problem; it constitutes the blueprint for the collection, measurement, and analysis of data. (Birhane, 2017, P.52)

In this study, the researcher used the mixed method i.e., the combination of the quantitative and qualitative research design. The reason for choosing both quantitative (validation) and

qualitative (inquiry) studies was that a single method can never adequately shed light on a phenomenon. (ibid., 2017)

According to Walliman (2011, P.72) qualitative research is based on data expressed in the form of words- descriptions, accounts, opinions, feelings etc. rather than numbers. This type of data is common whenever people are the focus of the study, particularly in social groups or as individuals.

Quantitative analysis deals with data in the form of numbers and uses mathematical operations to investigate their properties. The levels of measurement used in the collection of data are important factor in choosing the type of analysis that is applicable, as is the numbers of cases involved. Statistics is the name given to this type of analysis. Walliman (2011, P.72). For this study, therefore, it is important and appropriate to apply quantitative research method to harmonize findings of the interview (qualitative data).

3.3. Data Sources

The sources of the data were sampled from staff members of AATMA from the Department of Road Safety Education and Capacity Building Directorate, staff members of Addis Ababa Traffic Police (AATP) as well as from pedestrians of Addis Ababa. The reason for selecting employees of AATMA and AATP is that they are front liners and professionals having relevant experience and information in road safety awareness-raising activities. They are also relevant sources for the status of road traffic crashes in the City. Pedestrians were selected for being the major victims of road traffic crashes. (See appendix K)

3.4. Instruments of Data Collection

The research made use of two sets of data source- primary and secondary (documents) data. Primary data was obtained directly from respondents by questionnaire, in-depth interview and focus group discussions. In other words, primary data provided direct evidence or testimony about the subject under investigation. Secondary sources offered the researcher with second hand information which is essential for the thesis. Secondary sources include books, journal articles, periodic newspapers and magazines, proclamations, meeting minutes, speeches, reports, declarations, published and unpublished literatures, thesis, recorded audio and video materials and brochures. (Thomas, 2016, P. 41)

3.4.1. Questionnaire

As cited by Birhane (2017, P.55) James, W. (2003, P.237) explained that questionnaire is a systematic compilation of questions that are submitted to a sampling of population from which information is desired. It is a popular method of data collection instrument in quantitative research. Basically, it consists of a series of questions designed to obtain factual information or information about people attitude, value, opinions or beliefs about a particular subject or issue.

Based on the above explanations, the researcher designed the questionnaire in both open and closed ended questions for 141 respondents to get reliable information on the issue under discussion.

3.4.2. Individual in-depth Interview

According to Birhane (2017, P.56) in-depth interview is applied when immense information is expected to acquire from few people having special knowledge about the subject under study.

Therefore, to achieve the intended objectives of the study i.e., showing the activities, contributions, strong points and weaknesses as well as challenges of the awareness-raising interventions in the reduction of road traffic crashes, individual in-depth interview with three higher officials of the Education and Capacity Building Directorate of the Agency was conducted.

3.4.3. Focus Group Discussion (FGD)

Focus groups can be seen as a type of group interview, but one that tends to concentrate in depth on a particular theme or topic with an element of interaction. The group is often made up of people who have particular experience or knowledge about the subject of the research, or that have a particular interest in it. (Walliman, 2011, P.100)

The FGD of this research aimed at showing the activities, contribution, strong points and weaknesses as well as challenges of the awareness-raising interventions in the reduction of road traffic crashes. It was conducted at work place. The group members comprise five road safety experts at the Agency who have good background knowledge and information about the road safety awareness-raising efforts of the Agency.

To sum up, both qualitative and quantitative approaches were employed as data collection instruments because such instruments help combine the strengths and possible inadequacies

of any of the sources of data. Hence, questionnaires, interviews, and focus group discussions were used in the study.

3.5 Data Gathering Procedure and Piloting Instruments

According to Asemah et.al.in Birhane (2017, P.54) explain that survey is the most appropriate method of gathering and measuring data relating to demographics, attitude, opinion and perception. As cited by Birhane (2017, P.54), Wimmer and Dominick (2006) explain that data collection method is a specific data collection process in accordance with the assumption of the selected methodology. Therefore, in this study the researcher applied among other information gathering tools survey (both close-ended questionnaire with a few open-ended items) and individual in-depth interview. The questionnaires were prepared for three clustered group: employees of AATMA, AATP officers and pedestrians of Addis Ababa. The questionnaire for employees of AATMA has 16 questions centered around the overall perception of employees about the road safety awareness-raising efforts being done by the Agency as well as its cooperation with stakeholders to reduce road traffic crashes in the City. The questionnaire for AATP officers has 10 close-ended questions as well as some open-ended questions which try to get data about AATP officers' reaction about the road safety awareness-raising activities of the Agency. The third questionnaire for pedestrians of Addis Ababa is related to the knowledge they have about the Agency and its road safety awareness-raising efforts. The knowledge of pedestrians about the Agency can be a key point to estimate public attention to the duties of the Agency. Except the questionnaire to employees of AATMA, all questionnaires, interview and FGD questions were translated to Amharic.

Data collecting instruments were piloted to check whether they can generate the expected information and consider their internal consistency. In this respect, the instruments were given to the advisor and to another two individuals (a lecturer at a university and an assistant professor of linguistics) to judge the content validity and test for reliability. Thus, valuable advice was solicited from these professionals. Finally, the instruments were improved based on suggestions and recommendations gathered.

3.6. Methods of Data Analysis and Presentation

The qualitative data obtained through questionnaire, in-depth interview and FGD were analyzed and presented by narrative form whereas quantitative raw data from questionnaire were analyzed using descriptive and inferential statistics with the help of computer software

called Statistical Package for Social Sciences (SPSS) version 20. Tableau and bar chart presentation were used to present quantitative data.

3.7. Methods of Samplings

AATMA consists of five central departments as well as five branch offices operating in different parts of the City. All departments are committed to ensuring smooth traffic flow free from congestion and improving safety for all road users.

In this study, the researcher used purposive sampling and stratified sampling techniques. According to Kothari (2004, P.5) in Birhane (2017, P.54) purposive sampling (also known as judgment, selective or subjective sampling) is a sampling technique in which the researcher is self-reliant on his or her own judgment in choosing members of population to participate in the study.

Accordingly, the researcher used purposive sampling technique to select respondents for in depth interview and FGD participants to show the activities, contribution, strong points and weaknesses as well as challenges of the awareness-raising interventions in the reduction of road traffic crashes. Hence, 3 experts of the department who had updated information on the awareness-raising activities were purposefully selected because they were expected to give reliable information to interview. Besides, 5 experts from same department were purposely selected for the focus group discussion. Similarly, the researcher employed purposive sampling technique to gather data from employees of the Agency and members of AATP by using questionnaire for same purpose. In addition to showing the activities, contribution, strong points and weaknesses as well as challenges of the awareness-raising interventions in the reduction of road traffic crashes, questionnaire for AATP aimed at investigating the level of stakeholders' cooperation in the awareness-raising efforts. Thus, all members of the Department of Road Safety Education and Capacity Building Directorate as well as all members of Road Safety and Capacity Building Teams of the Branch Offices of the Agency (i.e. 41 respondents) and 50 members of AATP were purposively selected.

Moreover, the researcher used stratified sampling technique. NC State University defines, "stratified" means choosing from various sub-groups. The population is divided into subpopulations and random samples are taken of each subpopulation. Hence, the researcher used stratified sampling technique to choose respondents from pedestrians of Addis Ababa for questionnaire. Pedestrians were randomly selected based on population breakdown of two

Addis Ababa sub-cities (Bole and Yeka) and these respondents were grouped into five categories: shopkeepers, laborers, religious fathers, students and professionals. In sum, 152 respondents were subjects of the whole research.

3.8. Research Site

The research was conducted on the interventions of awareness- raising to reduce road traffic crashes by the Addis Ababa City Administration Traffic Management Agency. Hence, the area of study is Addis Ababa where the Agency as well as other data sources is located.

3.9. Ethical Considerations

During the course of this study, it was expected that there were some ethical questions to consider. The researcher respected the privacy of all questionnaire respondents and pledged them that it was not to be disclosed and not exposed to anyone. It will only be for the purpose of the research and there will not be names on the questionnaires and other unique identity of the participants will not be revealed. Besides, they were informed that if they were not comfortable to answer some of the questions, they would not be forced to answer. To conduct interview and FGD, the researcher asked their willingness to tape record the information and get hold of their photograph. They expressed their willingness. But with respondents from AATP, neither tape record nor photographing was allowed for the very reason that the military protocol does not allow to do so.

CHAPTER 4: RESULT AND DISCUSSION

This chapter deals with analysis, interpretation and discussion of the data gathered through questionnaire, FGD and in-depth interview related to effectiveness of awareness-raising activities to reduce road traffic crashes.

The data were collected and analyzed using both methods: qualitative and quantitative method.

4.1. Presentation of Quantitative Results

PART I

4.1.1. Employees' perception towards awareness-raising activities performed by the Agency, strong points as well as weaknesses noticed

In this part, the study tries to reveal the overall perception of employees towards the Agency's road safety awareness-raising efforts.

Table 1. Employees' perception towards awareness-raising activities performed by the Agency

Alternatives	Frequency	Percent
Agency regularly undertakes studies on awareness-raising campaigns		
Strongly agree	9	22
Agree	19	46.3
Medium	9	22
Disagree	4	9.8
Strongly disagree	--	--
Total	41	100
Agency takes essential evaluation on each		

awareness-raising activities		
Strongly agree	10	24.4
Agree	11	26.8
Medium	15	36.8
Disagree	5	12.2
Strongly disagree		
Total	41	100
Agency implements road safety awareness-raising education programs in a planned and organized manner		
Strongly agree	14	34.1
Agree	14	34.1
Medium	8	19.5
Disagree	5	12.2
Strongly disagree	--	--
Total	41	100

Table 1 deals with judging employees' attitude related to awareness-raising efforts being carried out by the Agency to reduce road traffic crashes. It also shows the strong points and weak sides of the awareness-raising interventions. In connection with this, 9 questions were raised to respondent employees in order to judge their perception regarding awareness-raising activities.

The first question that Table 1 treats was posed to judge the view of employees whether the Agency carries out studies before entering into awareness-raising campaigns. It was stated as, "the Agency regularly undertakes studies on awareness-raising campaigns" This question was supported by 28 (68.3%) of respondents by rating "strongly agree" and "agree". However, it was rejected by 4 (9.8%) of respondents. This indicates that majority (68%) of respondents hold the view that the Agency regularly undertakes studies on road safety awareness-raising.

Again, employees were questioned, "the Agency takes essential evaluation on each awareness-raising activities and takes corrective actions to reduce road traffic crashes" This

question was supported by 10 (24.4%) and 11 (26.8%). In sum 21 (51.2%) of the respondents agreed that the Agency takes essential evaluation on each awareness-raising activities and takes corrective actions to reduce road traffic crashes by rating “strongly agree” and “agree”. However, still significant number of respondents 15 (36.6%) were not satisfied with the idea and described it “medium”. While 5 (12.2%) of the respondents rejected the idea. This generally shows that the Agency takes evaluation and corrective actions on the awareness-raising programs.

The next question employees were asked was, “the Agency implements road safety awareness-raising education programs in a planned and organized manner” with regard to this, 14 (34.1%) and another 14 (34.1%) of employee respondents have judged positively by rating “strongly agree” and “agree” respectively. In sum, 68.2% of these employees have agreed that the road safety awareness-raising education of the Agency is provided in a planned and organized manner. However, 8 (19.5%) of them held the opinion that the implementation of a planned and organized road safety awareness-raising education was “medium”. Therefore, the research result indicates that the Agency offers road safety awareness-raising education in a planned and organized way.

Table 2. Employees judgment about awareness- raising efforts of the Agency

Agency implements improved and revised road safety awareness-raising activities each year	Frequency	Percent
Strongly agree	8	19.5
Agree	17	41.5
Medium	9	22
Disagree	7	17.1
Strongly disagree		
Total	41	100
Contents of awareness-raising (education) of the Agency are related to behavioral and ethical problems of both drivers and pedestrians		

Strongly agree	17	41.5
Agree	12	29.3
Medium	10	24.4
Disagree	1	2.4
Strongly disagree	1	2.4
Total	41	100
Agency works with education sectors to include road safety and traffic management system in the curriculum		
Strongly agree	12	29.3
Agree	16	39
Medium	8	19.5
Disagree	5	12.2
Strongly disagree	--	--
Total	41	100

Another question still raised to employee respondents was whether the Agency implements improved and revised road safety awareness-raising activities each year. As one can observe from the Table, this question was favored by 8 (19.5%) and 17 (41.5%) of the respondents by rating “strongly agree” and “agree” respectively; whereas, 7 (17.1%) have expressed their disagreement. Hence, this indicates that the Agency’s road safety awareness-raising activities are getting improved and some revision is made. Employees were also questioned to comment on whether the contents of awareness-raising (education) of the Agency are related to behavioral and ethical problems of both drivers and pedestrians. The question was supported by 17 (41.5%) and 12 (29.3%) of the respondents by rating “strongly agree” and “agree” respectively. In sum, 29 (70.8%) of the respondents agreed that the contents of Agency’s road safety awareness-raising education address the behavioral and ethical problems of both drivers and pedestrians. This finding indicates that the great majority of the respondents judged that the contents of awareness-raising (education) of the Agency are related to behavioral and ethical problems of both drivers and pedestrians. The other question employees were asked was if the Agency is working with education sectors to include road

safety and traffic management system in the curriculum. This question was supported by 12 (29.3%) and 16 (39%) of the respondents by rating “strongly agree” and “agree” respectively. In sum, 28 (68.3%) of the respondents have agreed that the Agency is working with education sector to include road safety and traffic management in the curriculum.

Table 3. Employees view about Agency’s public engagement in awareness-raising efforts

Agency has specialized and scheduled awareness creation plan at public gatherings	Frequency	Percent
Strongly agree	17	41.5
Agree	18	43.9
Medium	6	14.6
Disagree	--	--
Strongly disagree	--	--
Total	41	100
Agency’s awareness creation considers the social status (gender, age, education, etc) of road users		
Strongly agree	8	19.5
Agree	11	26.8
Medium	17	41.5
Disagree	3	7.3
Strongly disagree	2	4.9
Total	41	100
There is active and strong public engagement in the Agency’s awareness-raising activities		
Strongly agree	6	14.6
Agree	14	34.1
Medium	17	41.5
Disagree	4	9.8
Strongly disagree	--	--
Total	41	100

Employees were questioned if the Agency has specialized and scheduled awareness creation plan at public gatherings (in schools, religious institutions, in market areas) by crafting appropriate messages. The vast majority of research participants 35 (85.4%) have appreciated the awareness creation of the Agency at public gatherings by rating “strongly agree” and “agree” 17 (41.5) and 18 (43.9%) respectively. While small portions 6 (14.6%) of them judged the awareness creation at public gatherings “medium”. From this point of view, we can deduce that the Agency is working at grassroots level to create awareness about road safety issues in a planned manner. Another question still raised to employee respondents was stated as, “to improve awareness creation and positive behavior of road users (pedestrians and drivers) the Agency’s awareness creation mechanisms take the social status (gender, age, education, etc.) of road users into consideration.” This question was favored by 8 (19.5%) and 11 (26.8) of the respondents by rating “strongly agree” and “agree” respectively. In sum, 19 (46.3%) have expressed their agreement that the Agency’s awareness creation strategy considers road users’ social status. But, for another significant number of respondents 17 (41.5%), such consideration of social status was “medium” while negligible number 5 (12.2%) rejected this idea. From this finding, we can judge that the Agency’s awareness-raising program to some extent considers social status.

The other question proposed was whether there is active and strong public engagement in the Agency’s awareness-raising activities. This question was supported by 6 (14.6%) and 14 (34.1%) of the respondents by rating “strongly agree” and “agree” respectively. In sum, 20 (48.7%) have positively favored this statement. While 4 (9.8%) of respondents rejected it. According to 17 (41.5 %) of the respondents, however, there is “medium” public engagement in the Agency’s awareness-raising activities.

4.1.2. Employees’ judgment about Agency’s media utilization performance to raise public awareness

Table 4. Employees’ perception towards Agency’s media utilization performance

Alternatives	Frequency	Percent
Agency uses multiple media channels to run awareness-raising campaigns as is expected		
Strongly agree	7	17.1
Agree	12	29.3

Medium	18	43.9
Disagree	4	9.8
Strongly disagree	--	--
Total	41	100
Agency appropriately utilizes local broadcast media		
Strongly agree	6	14.6
Agree	14	34.1
Medium	15	36.6
Disagree	6	14.6
Strongly disagree	--	--
Total	41	100
Agency appropriately utilizes local print media		
Strongly agree	8	19.5
Agree	17	41.5
Medium	12	29.3
Disagree	4	9.8
Strongly disagree	--	--
Total	41	100
Agency has continuous awareness-raising programs on media		
Strongly agree	6	14.6
Agree	11	26.8
Medium	16	39
Disagree	6	14.6
Strongly disagree	2	4.9
Total	41	100

As shown in Table 4, the questions proposed were related to the Agency's utilization of media to develop public awareness on road safety. Thus, respondents were asked if the Agency uses multiple media channels to run awareness-raising campaigns as is expected. Pertaining to this question, 7 (17.1%) and 12 (29.3%) in sum 19 (46.4%) of the respondents have rated "strongly agree" and "agree" respectively. But, 4 (9.8%) rejected this statement. 18 (43.9%) of them rated the use of multiple media channels by the Agency "medium". From this point of view, we can infer that the Agency is not utilizing multiple media channels at

full scale. Additionally, employee respondents were asked, “as part of its awareness-raising programs, the Agency appropriately utilizes local broadcast media” This question was supported by 6 (14.6%) and 14 (34.1%) in sum, 20 (48.7%) of respondents by rating “strongly agree” and “agree” respectively. Again, respondents were asked to judge the Agency’s appropriate utilization of local print media. Accordingly, this question was positively favored by 8 (19.5%) and 17 (41.5%) in sum, 25 (61%) of the respondents by rating “strongly agree” and “agree” respectively. Besides, respondents were questioned about the continuity of the awareness-raising programs of the Agency on media. 6 (14.6%) and 11 (26.8) of the respondents in sum, 17 (41.5) have expressed their agreement by rating “strongly agree” and “agree” respectively. Therefore, from this research finding, one can imagine that the Agency is not utilizing local broadcast and print media at optimum level. In connection with this, scholars mention that one of the strategies of awareness-raising programs is using media.

4.1.3. The cooperation of stakeholders in the Agency’s awareness-raising activities

Table 5. Attitude of employees towards stakeholders’ participation in Agency’s awareness-raising programs

Alternatives	Frequency	Percent
Agency carries out capacity building activities on transport sectors and traffic police knowledge		
Strongly agree	6	14.6
Agree	24	58.5
Medium	8	19.5
Disagree	3	7.3
Strongly disagree	--	--
Total	41	100
Agency has experience sharing programs with stakeholders		
Strongly agree	9	22
Agree	15	36.6
Medium	10	24.4
Disagree	6	14.6
Strongly disagree	1	2.4

Total	41	100
Agency provides awareness-raising (education) on traffic laws and regulations to drivers and pedestrians		
Strongly agree	9	22
Agree	18	43.9
Medium	12	29.3
Disagree	2	4.9
Strongly disagree	--	--
Total	41	100

Table 5 describes employees' attitude towards stakeholders' participation in Agency's awareness-raising programs. In relation to this, one of the questions raised was, "to prevent road traffic crashes, the Agency carries out capacity building activities on transport sectors and traffic police knowledge". This question was supported by 6 (14.6%) and 24 (58.5%) employee respondents. In sum, 30 (73.1 %) have agreed by rating "strongly agree" and "agree" respectively. From this finding, we can deduce that the Agency is doing capacity building activities to those operating in transport sector and traffic police because we can imagine that road traffic crashes can be reduced by a joint action and participation of all actors in the area. This has to be supported and maintained.

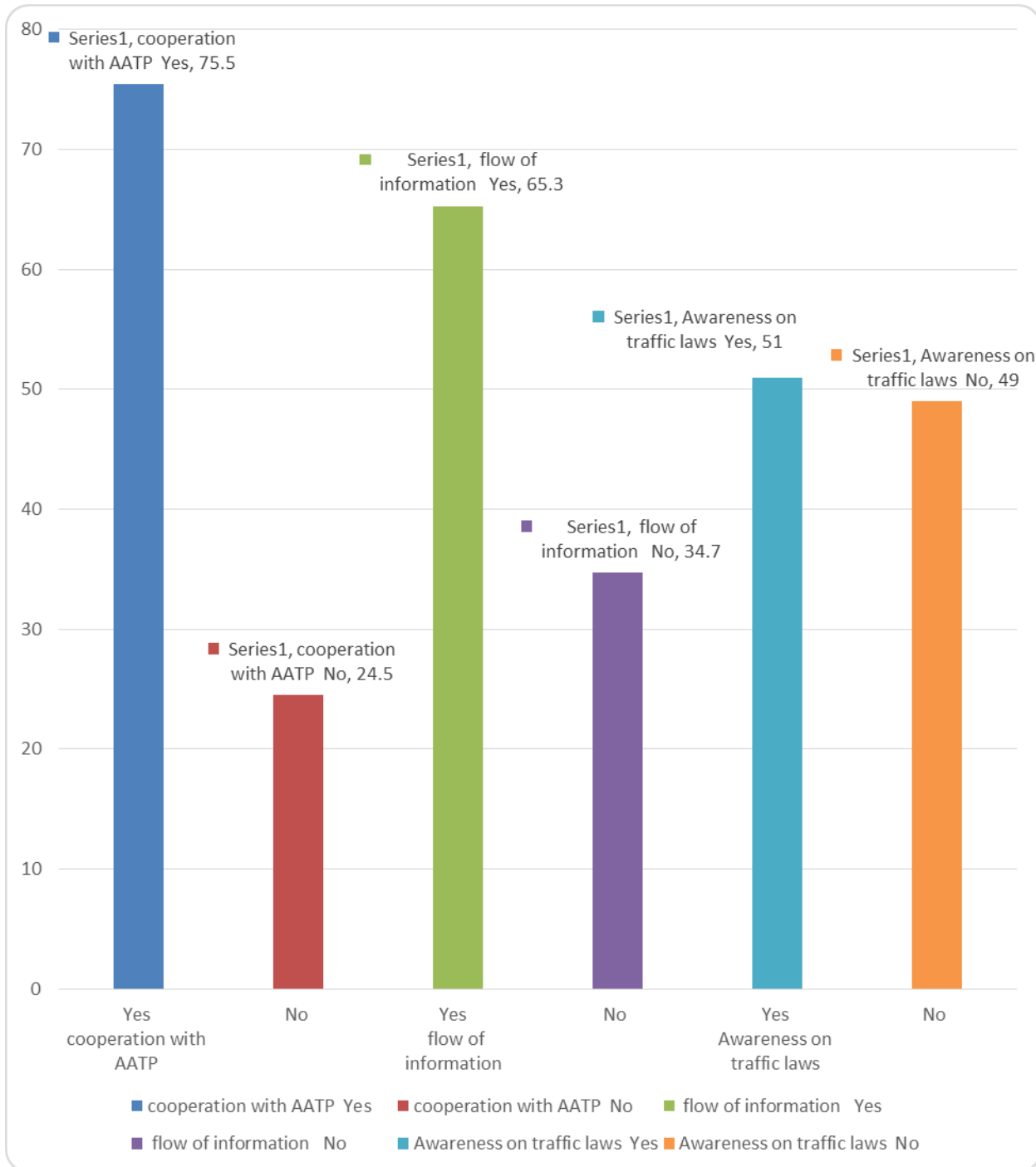
By its nature, road traffic crashes can be reduced by a joint effort or cooperation of all actors operating in the sector in particular and the society in general. The researcher has aimed to measure the Agency's cooperation with its stakeholders. Thus, respondents were asked if staffs of education and capacity building directorate of the Agency have experience sharing programs with stakeholders on how to conduct awareness-raising programs. In connection with this question, 9 (22%) and 15 (36.6%) respondents, in sum, 24 (58.6%) respondents agreed that the Agency undertakes experience sharing program by rating "strongly agree" and "agree" respectively. From this point of view, we can understand that there is experience sharing with stakeholders but it takes place in an inadequate frequency.

The last but not the least question forwarded to the respondents is, "the Agency provides awareness-raising (education) on traffic laws and regulations to drivers and pedestrians on regular basis" Pertaining this question, 9 (22%) and 18 (43.9%) of the respondents have supported this statement by rating "strongly agree" and "agree" respectively. In sum, 27 (65.9%) of the respondents have positively favored this statement. From this result, one can

understand that the Agency provides awareness-raising (education) on traffic laws and regulations to drivers and pedestrians on regular basis.

PART II

Chart 1. Reaction of members of AATP to awareness-raising activities of the Agency and its relationship with AATP



Charts 1-3 indicate reaction of traffic police respondents to the questions raised in relation to the awareness-raising activities of the Agency and its overall cooperation with AATP. These questions were raised to know the level of cooperation between the Agency and its strong and all-time ally- AATP in the fight against road traffic crashes. AATP is an active stakeholder of the Agency as confirmed by interview key informants. We know that reduction of road traffic crashes is unattainable without active involvement or cooperation of stakeholders in the sector.

In this connection, traffic police respondents were questioned whether there is good cooperation between the Agency and AATP to reduce road traffic crashes. As we can see from the above chart, majority 37 (74%) of the respondents confirmed that the Agency is working jointly with AATP to reduce road traffic crashes. Whereas, some of the respondents 12 (24%) rejected it. This indicates that the Agency has good ties with AATP to reduce road traffic crashes but the cooperation should be strengthened and disclosed. The next question was about the flow of information (communication) between Addis Ababa Traffic Police and the Agency concerning road traffic crashes. From the above chart, we can see that 32 (64%) of the respondents replied there is information flow or communication between the Agency and Addis Ababa Traffic Police. From this data analysis, we can judge that there is good communication or information exchange between the Agency and AATP. Traffic respondents were asked about capacity building trainings offered by the Agency. The question reads, “The Agency gives awareness-raising (education) on traffic laws and regulations to law enforcing organs like traffic police in organized way.”

Pertaining to this question, majority 28 (56%) of the respondents supported the question by remarking “yes” while significant number of the respondents 21(42%) rejected it. Therefore, from this finding, we can explain that the Agency gives awareness-raising or education on traffic laws and regulations to members of AATP.

Chart 2. Agency Cooperation with AATP

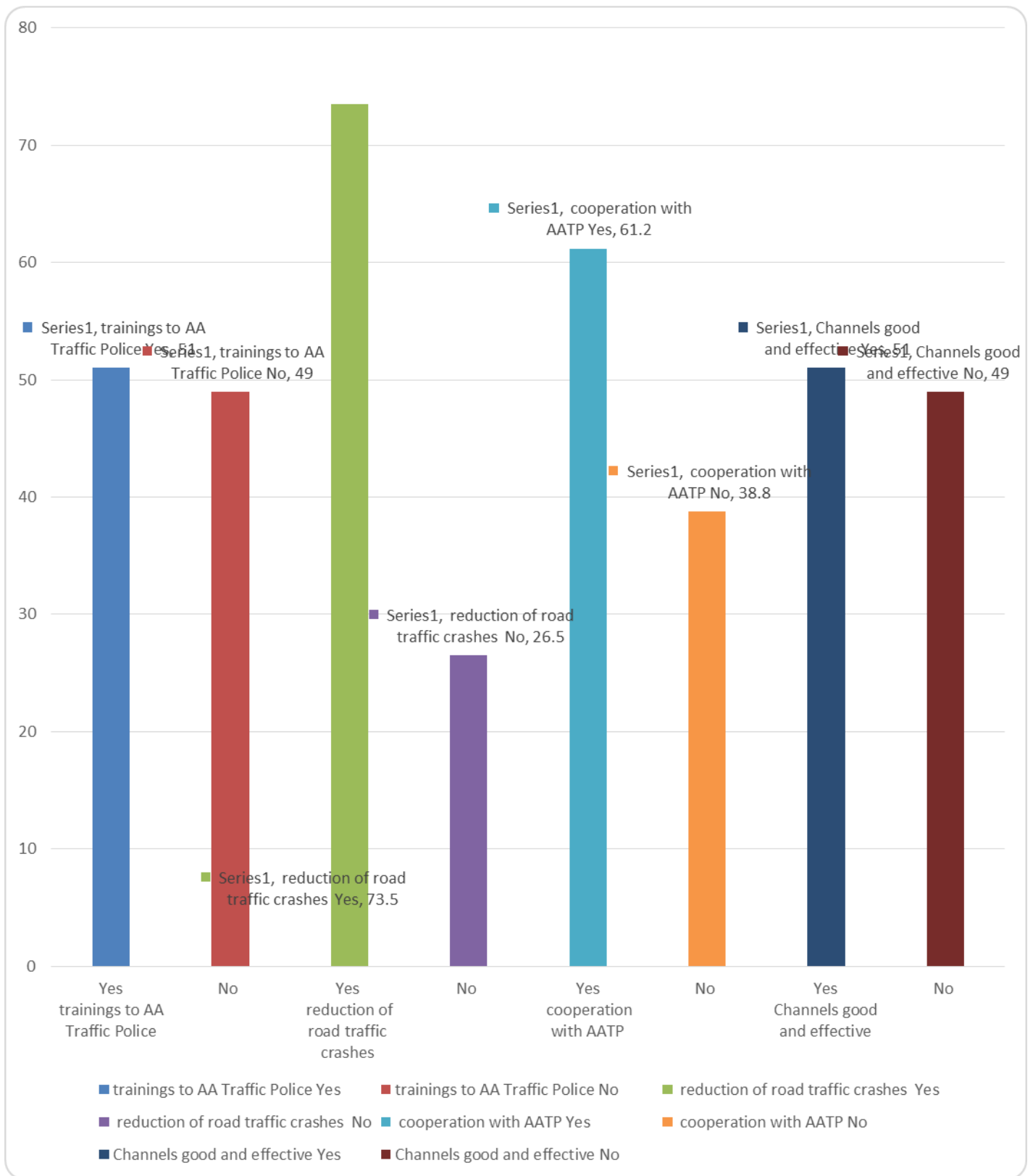
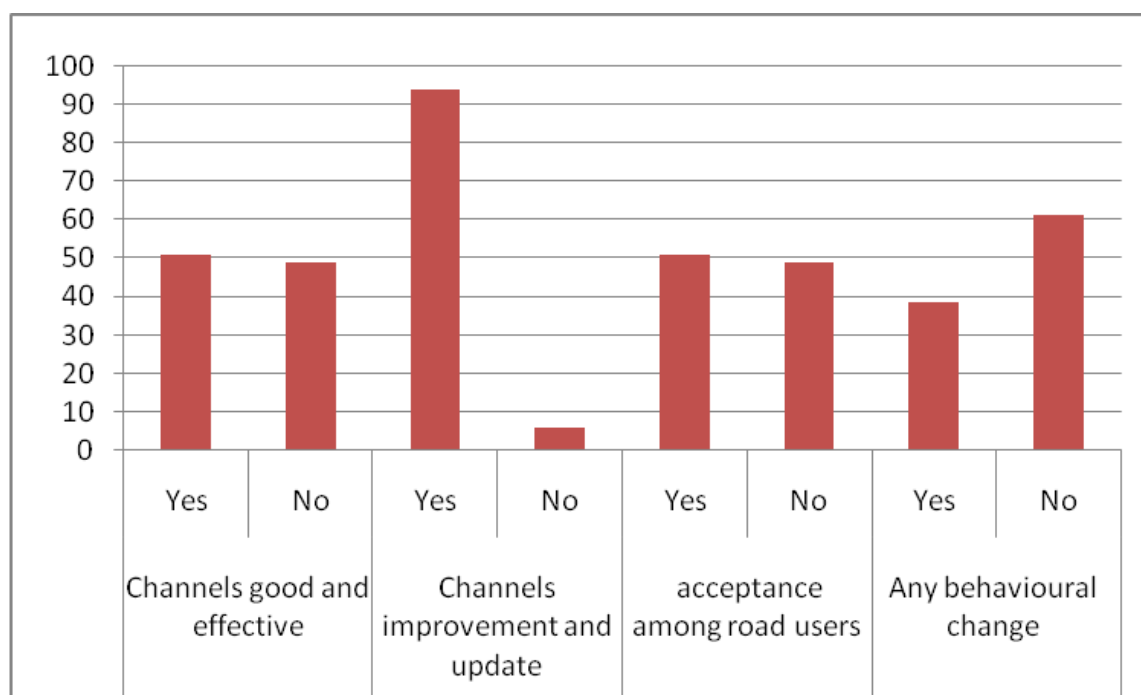


Chart 2 entertains similar question to the above one, “the Agency organizes capacity building trainings to Addis Ababa Traffic Police to support the effort of reducing road traffic crashes.” Majority of the respondents 25 (50%) favored this statement positively by rating “yes” while another significant number of the respondents 24 (48) rejected by rating “no”. From this finding, we can say that the Agency somehow tries to offer capacity building trainings to the traffic police but not all traffic police members are taking part in the training. Concerning the role of the awareness-raising programs in the decline of traffic crashes in the last two years, 36 (72%) of respondents confirmed that the awareness-raising programs of the Agency played key role in the reduction of traffic crashes; while, 13 (26%) replied Agency’s role did not contribute to the result registered. Another question still raised to traffic police respondents was stated as, “Does the Agency work awareness- raising activities in cooperation with AATP?” Regarding this question majority of the respondents 30 (60%) commended the cooperation between the Agency and AATP to do road safety awareness-raising activities. From this point of view, one can understand that the awareness-raising programs have contributed a lot to the reduction of road traffic crashes. From this finding, we can also judge that the Agency undertakes awareness-raising campaigns in cooperation with AATP.

The above chart also discusses if the channels of awareness-raising programs are good and effective. Here half of the respondents 25 (50%) replied the Agency’s channels of awareness-raising programs are good and effective. But, almost another half 24 (48%) of the respondents believed the channels are not effective.

Chart 3. Outlook of members of AATP about Agency communication methods



In connection with the channels, Chart 3 highlights whether the channels require some improvements and updates. Here almost all respondents 46 (92%) replied the channels should be improved and need updates. Therefore, from respondents' view we can judge that the Agency's channels of awareness-raising programs cannot be entirely described effective; they require some improvements or adjustments.

Additionally, respondents were asked to say their say on the acceptance of the Agency's awareness-raising activities among road users. The vast majority of the respondents 35 (70%) believed the Agency's awareness-raising activities have good acceptance among road users. While 14 (28%) of the respondents rejected this statement.

To find out the clue for the effectiveness of the Agency's awareness-raising activities to reduce road traffic crashes, respondents were asked whether they noticed any behavioural change on pedestrians and car drivers as a result of the awareness-raising activities. For majority of the respondents 30 (60%) the awareness-raising activities have resulted in bringing about little or no behavioral change. However, 19 (38%) of the respondents have positively favored this statement and believed pedestrians and drivers have shown behavioral changes due to the awareness-raising activities. From the finding of this data as well as from our prior knowledge about the point under discussion i.e. attainment of behavioral changes,

despite some improvements, it is difficult to develop behavioral changes within short period of time as the Agency is five years age.

PART III

Table 6: View and action of Addis Ababa Pedestrians to the questions related to awareness-raising activities of AATMA

Alternatives	Frequency	Percent
Do you have sufficient knowledge about some of the duties of the Agency		
Yes	29	58
No	21	42
Total	50	100
Do you use the print media?		
Yes	23	46
No	27	54
Total	50	100
Do you use the electronic media		
Yes	21	42
No	29	58
Total	50	100
Do you pay sufficient attention to the awareness-raising activities of the Agency?		
Yes	19	38
No	31	62
Total	50	100
Do the awareness-raising activities of the Agency have positive impacts on pedestrians' road using behavior?		
Yes	23	46
No	27	54
Total	50	100
Does the Agency do its label best to reduce road traffic crashes?		
Yes	32	64
No	18	36
Total	50	100

Table 6 discusses the reaction of pedestrians of Addis Ababa to questions related to the awareness-raising activities of the Agency and general reflections of these pedestrians about

the Agency. This is due to the fact that all activities of the Agency aim at serving road users with a mandate of ensuring safe and acceptable traffic movement in the City by informing proper road use rules among other things. Therefore, it is crucial that pedestrians as a prime focus of service rendering activities of the Agency are expected to have good knowledge about it.

Henceforth, the Agency has to assess the know-how and view of pedestrians about each of its activities particularly its education or awareness efforts. Pertaining to this, pedestrian respondents were asked if they know some of the duties of the Agency. Majority of the respondents 29 (58%) replied they are knowledgeable about the mandate of the Agency. However, significant number of the respondents 21 (42%) explained they do not know the duties and the responsibilities of the Agency. These findings show that further promotional and out reaching activities have to be done by the Agency so that all its prime targets, pedestrians will have knowledge about it.

The other focus area of Table 6 discusses status of pedestrians' utilization of media. Accordingly, pedestrians were questioned if they utilize the print media of the Agency about road safety awareness-raising issues. Majority of the respondents 27 (54%) rejected this statement by saying "no". But 23 (46%) of them responded they utilize the print media. Similarly, respondents were questioned same question about the electronic media (TV, face book, twitter etc.). Still 29 (58%) replied "no" and only 21 (42%) of the respondents answered they use the electronics media of the Agency. This shows that more effort is needed to utilize the media and the media have to become more accessible to the community.

Another question presented to the pedestrians is related to the attention of pedestrians about the awareness-raising activities of the Agency. Majority of the respondents 31 (62%) disclosed that they do not pay attention to the awareness-raising activities of the Agency. While 21 (42%) responded positively to this statement. From this data, we can deduce that the Agency has to make its awareness-raising activities attractive and catching.

Respondents were questioned if the road safety awareness-raising activities of the Agency have had positive impacts on their road using habit or behavior. More than half of the respondents 27 (54%) replied that the awareness-raising activities have not had positive impacts on their road using behavior. On the other hand, 23 (46%) have positively favored this statement. From this point of view, we would like to show that the Agency has to

regularly conduct impact assessment on its awareness-raising activities and take corrective measures.

Concerning the status of the Agency's awareness-raising programs to reduce road traffic crashes, 32 (64%) respondents commended the Agency is doing its label best to reduce road traffic crashes through its awareness-raising programs. While 18 (36%) respondents replied such activities of the Agency are not at the utmost effort. This shows that so far the Agency has performed commendable activities to reduce road traffic crashes through its awareness-raising programs; however, there are some tasks to be done ahead of time.

4.2. Description of Qualitative Results

This section of the chapter deals with only qualitative data gathered through FGDs and in-depth-interview.

Among the primary causes of road traffic crashes, human error is in the fore front line. Key informants divulged the role of awareness-raising activities in combating the problem in the following way:

At global level, the risk factors of road traffic crashes include: over speed, drink and driving and violating traffic rules among other things. Generally speaking, all these risk factors are caused by human error- man made problems. To reduce and gradually eradicate road traffic crashes, we have to work on human capacity and behavioral changes. This requires conducting continuous awareness-raising campaigns by providing information, education to road users. This idea has been substantiated by a statement from European Commission (2005, P. 8) as, "one has to keep in mind that effective RSE includes a key element which none of the other road safety key interventions, like engineering, enforcement or licensing training covers. This key element is creating and changing attitudes towards safe and socially responsible behavior in traffic."

Regarding the decline in road traffic crashes in the last two years, respondents were asked to reflect on the main factor in general and the contribution of the awareness-raising program in particular.

Deputy Director General of Addis Ababa City Administration Traffic Management Agency, Birhanu Girma disclosed his view in the interview session by saying,

“The awareness-raising activities are carried out so as to reduce traffic crashes. Awareness-raising activities singly may not be effective to reduce road traffic crashes but it will be effective if it is done in combination with engineering and enforcement solutions. As a result of such combined actions, improved results have been registered. For instance, up to the year 2018, road traffic fatality was increasing by 5% then it was possible to control it not to exceed this level. In the aftermath, the percentage was reduced to 7%. Moreover, if we compare the ninth month performance report of this budget year (2020/2021) with that of last year, traffic fatality declined to 18 %. Therefore, this indicates that extensive and effective awareness-raising activities have been undertaken.”

Forwarding her point of view, Director of Education and Capacity Building Directorate of the Agency, Genet Dibaba, explained, “Exhaustive efforts have been made to enhance communication modes to reach the public. To mention some: launching field campaign against road crashes, giving education at public institutions like faith-based organizations, schools, health centres among other things.

Majority of the FGD informants unveiled the commitment and attention of the City Administration for road safety has played significant role for the decline of road traffic fatalities. It is through the will and action of the City Administration that the Agency was established and from then on extensive engineering, education and enforcement activities could be put in action. Such decisions and actions contributed a lot to the results achieved.

In addition, FGD participants remarked awareness-raising activities have played key role for the decline of road traffic fatality; these activities should go beyond sharing information but not yet at the level of bringing behavioral change. The desired change can be realized if we work on beyond sharing information and arrive at the level of making people responsibly obey traffic rules. To reach this standard, we have to undertake repetitive and progressive awareness-raising activities and, in the meantime, we have to evaluate our messages and style of communication.

In the interview session Mr. Berhanu disclosed that the community is reluctant to obeying traffic laws and rules when moving from one place to another. This indicates that there are a lot of more tasks to be done again on awareness-raising. In fact, for the last two years, the

Agency has undertaken quite several awareness-raising activities more than any time but still not enough. The Agency can perform more than this provided that enabling and suitable atmosphere is created.

Moreover, respondents were questioned to mention some of the strengths, weaknesses and challenges encountered in the awareness-raising intervention. Thus, they mentioned the following points:

Tracing some of the strengths of the awareness-raising programs, Mr. Hagere Hailu, Agency's Road Safety Capacity Building Team Leader forwarded in the interview session that previously, the Agency has been using the broadcast media both electronic and print. Now it has come to use various community forums as well which played significant role in having participatory public engagement and accessing the various segment of the community. Moreover, the effort to use mini- media at schools before and after class by using student traffic can be mentioned fruitful and rewarding.

According to Mrs. Genet as is common in many team actors, not all stakeholders are active participants in some duties that require joint efforts. Lack of monitoring awareness-raising communication messages (billboards) being displayed in different parts of the City could be considered another weakness noticed in awareness-raising activities so far.

Genet added that some of the major challenges of the awareness-raising activity include under the existing financial regulations of the City, it has been difficult to fully utilize the media and become more accessible to the public. Likewise, budget constraints and high cost of the media have greatly impacted the awareness-raising activity of the Agency. Lack of earned media and passive participation of some stakeholders can be considered another challenge.

The other key challenge pointed out in the interview session was that the Agency is giving more emphasis/attention to engineering than road safety education and enforcement.

FGD respondents stated that to meet its institutional responsibilities, the Agency is structured in such a way that its pillars of traffic management have equal authority and accountability. In accordance with its pillars, therefore, the three departments are at directorate level. This shows that at structural level, the Agency has given equal attention. However, this seems to be so only theoretically; the reality on the ground is quite different. The Agency is giving

more emphasis and priority to engineering and enforcement solutions than the awareness-raising efforts.

In connection with this question, the researcher has also tried to observe the stand of the Agency i.e., it strives to give priority to engineering from its three years traffic management strategy which states, “Engineering and technology play crucial role in traffic management. Every effort of the Agency must be supported by engineering and technology. Where possible always engineering and technology must be given priority. They are a means of creating a self-reliant road traffic system that doesn’t depend on the traffic police to operate. The Agency will create the capacity to utilize and implement technologies. It will seek engineering and technology solutions for traffic management problems.” Addis Ababa Traffic Management Agency Strategy (2019, P. 16). Despite such assertion, therefore, as they stand, the three aspects: education, engineering and enforcement are interdependent. One cannot be effective without the other.

With regard to additional efforts need to be made to enhance the road safety awareness-raising activities of the Agency, interviewees claimed so far the Agency could use a few but not all available local electronic media due to high cost and budget constraints. Although we are doing on social issues and public safety, we are required to pay equal cost with those running commercial advertisements. Some corrective action has to be taken. The upper management of the Agency has to work on it and move an amendment to the cost. Moreover, capacity building trainings have to be offered to employees of the Agency on how to undertake better awareness- raising programs.

In the move to combat road traffic crashes, it is clear that media are crucially important. Thus, key informants were asked to elaborate the communication mode and media platforms of the Agency.

According to interview informants the prime objective of the Agency is to ensure safe and acceptable traffic movement in Addis Ababa. To realize this objective, the Agency uses various modes of communication such as the broadcast media (both electronic and print media), and face-to-face awareness and education discussions. Currently, the Agency is working on these three modes of communication (electronic, print and face-to-face) in a very limited way. For instance, the Agency crafts short and touchy messages that are aired within a minute for pedestrians and drivers. The messages are very much related to the life situation

of all road users. The face-to-face discussion platform has been a good opportunity for a door-to-door approach to provide education and training to community leaders.

Besides, the Agency conducts baseline assessment on the achievements registered as a result of the awareness-raising programs. The assessment task examines the outcome of the awareness program and cross checks the behavioural change of road users whether they have taken improved road using habit and evaluates achievements both in the pre and post awareness-raising activities. Accordingly, the Agency has realized that the percentage of road traffic crashes have declined to 18% during the ninth month of this budget year. However, it is difficult to conclude that such result is achieved only through awareness-raising activities but due to the combined effort of awareness-raising, operational and enforcement activities.

CHAPTER 5: CONCLUSIONS AND RECOMMENDATIONS

This chapter is devoted to making conclusions on the basis of major findings of the study and then viable recommendations that can be used for future research on the topic will be made.

5.1. Conclusions

On the basis of the study results, the researcher has come up with the following conclusions. Nowadays, road traffic crash is claiming the life of millions of people across the globe. It is an abrupt incident that can happen to anyone at any time. Mostly, the victims are productive citizens. Man-made problems are the major factor for road traffic crashes. To combat this problem in Addis Ababa, Addis Ababa City Administration Traffic Management Agency is strategically operating on it. The Agency based on its pillar strategies has embarked on the three E's to manage road safety: education, engineering and enforcement. So far due to Agency's road safety intervention strategies (the combined effort of the three E's) remarkable achievements could be registered i.e., road traffic fatality is declining. However, the problem is still grave and needs further efforts. The engineering and enforcement efforts of the Agency can be more promising in the decline of road traffic crashes if these are complemented by awareness-raising (education) and inter-sectoral collaborations. However, the Agency is not giving equal attention to the 3E's i.e., the engineering effort has been its top priority.

The research aims at showing the contribution and strength of the awareness-raising interventions in the reduction of road traffic crashes and the public view towards awareness-raising activities. Thus, according to the findings of this study, some of the successful road safety awareness-raising efforts (strong points) of the Agency include: offering awareness-raising education mostly in a planned and organized way, preparing contents that address the behavioral and ethical problems of road users, working with education sector to include road safety and traffic management system in the curriculum, working at grass roots level i.e. having specialized and scheduled awareness creation plan at public gatherings (in schools, religious institutions, market areas), utilizing media to educate or inform the public.

The research findings of this study revealed that in the course of discharging its responsibilities, financial regulations of the City, budget constraints and high cost of the

media have greatly impacted the awareness-raising activity of the Agency i.e. challenges of the awareness-raising program.

5.2.Recommendations

Based on the major findings of the study and conclusions drawn, the following recommendations are suggested with the hope that they could be input and springboard for the road safety awareness-raising intervention of the Agency as well as for further similar studies.

- ❖ Aristotle, a Greek philosopher once said, “The roots of education are bitter, but the fruit is sweet” must be the motto of the education/ awareness-raising activities of the Agency. Awareness-raising as stated earlier is not an easy task full of difficulties but it will be finally rewarding. Through deep commitment and dedication to the awareness-raising program, we save others and ourselves. Therefore, despite the challenges, the Agency should provide repetitive and continuing road safety awareness-raising education.
- ❖ Media are believed to be key strategy and upper hand of awareness-raising strategies. The finding of this study revealed that due to budget constraints and harsh financial rules, the Agency is not fully utilizing the electronic media. This can hamper the potential effectiveness of the road safety awareness programs. To conduct up-to-date and better road safety awareness-raising programs using the media, the City Administration has to allocate sufficient budget for awareness-raising programs.
- ❖ The awareness-raising activity is done to serve the public i.e., it is a social issue. To utilize media, however, the Agency is required to pay equal cost with those running commercial advertisements. Media houses and other concerned bodies have to take corrective action. The upper management of the Agency has to remain engaged in lobbying media houses and other concerned bodies to largely discount road safety program costs as road safety issue is a common social responsibility.
- ❖ The Agency has to regularly conduct impact assessment on its awareness-raising programs.
- ❖ The Agency has to offer capacity building trainings to road safety professionals on how to organize awareness- raising programs.

- ❖ With regard to the effort being undertaken to make road safety education visible in curricula, among important facts to be considered include only for those subjects which are part of the curriculum, adequate training should be provided for teachers and other key players, such as police officers, and maintained in the long run European Commission (2005, P.13)
- ❖ Besides, there should be a need for the establishment of coordination bodies, networks or platforms and the identification of active contact persons at all levels in all institutions involved European Commission (2005, P.11)
- ❖ Road safety management of the Agency comprises the 3 E's: education, engineering and enforcement. Practically, the three aspects become effective if they work in combination. However, in its three-year strategy plan, the Agency asserts that if conditions allow, engineering must be given priority. However, more or less equal emphasis should be given to the three road safety management because human behavior is decisive but neglected in the course of combating road traffic crashes.
- ❖ The three road safety aspects can be described as the triangle of road safety. As a triangle has equal angle, for the three road safety aspects become effective, the Agency should give equal emphasis.

5.3. Suggestions for Further Research

Road safety education is a critical issue to reduce road traffic crashes. Efforts are being made to ensure safe traffic flow most notably in the areas of engineering, education and enforcement. As indicated in the findings of this research, despite its severe consequences, road traffic crashes can be controlled and avoided as it is mainly caused by human error. As a human error, education (awareness-raising) can be a good strategic response to eradicate risk behaviour. In Ethiopia, on average 13 people lose their life due to road traffic crashes each day. Therefore, the researcher suggests if a study is conducted on:

Impact assessment on awareness-raising programs

Comparative analysis of road safety awareness-raising activities of two regions of Ethiopia (Amhara and Oromia or SNNPRS and Afar etc.)

Analysis of national status of road safety awareness-raising activities

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APPENDIX A

ADDIS ABABA UNIVERSITY

GRADUATE SCHOOL OF JOURNALISM AND COMMUNICATION

Questionnaire for Staff Members of Addis Ababa Traffic Management Agency (AATMA)

Dear respondents,

The purpose of this questionnaire is to collect data in order to investigate the effectiveness of awareness-raising activities to reduce road traffic crash in Addis Ababa. Your genuine and accurate responses will no doubt have great contribution to the findings of the research. Therefore, you are kindly requested to give genuine answers to the questions below. The researcher would like to assure you that the answers that you give will be used only for the research purpose. For this, I would like to thank you in advance.

Instruction

1. You do not need to write your name or identity.
2. Please provide short answers to each question and by circling the answers for choice questions.

Section I

Table 1: Employees' reaction to the questions related to awareness-raising activities of AATMA

Note: SA= strongly agree A= agree M= medium D= disagree SD= strongly disagree

No	Categories					
		SA	A	M	D	SD
1.	The Agency (AATMA) undertakes studies on awareness-raising campaigns regularly					
2.	The Agency takes essential evaluation on each awareness-raising activities and takes corrective actions to reduce road traffic crashes					

3.	The Agency implements road safety awareness-raising education programs in a planned and organized manner					
4.	The Agency uses multiple media channels to run awareness-raising campaigns					
5.	The Agency fully utilizes local broadcast media					
6.	The Agency fully utilizes local print media					
7.	The Agency has continuous awareness-raising programs on media					
8.	Staffs of education and capacity building directorate have experience sharing programs with stake holders on how to conduct awareness-raising programs					
9.	The Agency provides awareness-raising (education) on traffic laws and regulations to drivers and pedestrians on regular basis					
10.	To prevent road traffic crash, the Agency carries out capacity building activities on transport sectors and traffic police knowledge					
11.	There is active and strong public engagement in the Agency's awareness-raising activities					
12.	The Agency implements improved and revised road safety awareness-raising activities every new year.					
13.	The contents of awareness-raising (education) of the Agency are related to behavioral and ethical problems of both drivers and pedestrians					
14.	The Agency is working with education sectors to include road safety and traffic management system in the curriculum					
15.	The Agency has specialized and scheduled awareness creation plan at public gatherings (in schools, religious institution, in market areas) by crafting appropriate messages					
16.	To improve awareness creation and positive behavior of road users (pedestrians and drivers) , the Agency's awareness creation mechanisms take the social status (gender, age, education, etc) of road users into consideration					

APPENDIX B

ADDIS ABABA UNIVERSITY

GRADUATE SCHOOL OF JOURNALISM AND COMMUNICATION

Questionnaire for members of Addis Ababa Traffic Police

Dear respondents,

The purpose of this questionnaire is to collect data in order to investigate the effectiveness of awareness-raising activities to reduce road traffic crash in Addis Ababa. Your genuine and accurate responses will no doubt have great contribution to the findings of the research. Therefore, you are kindly requested to give genuine answers to the questions below. The researcher would like to assure you that the answers that you give will be used only for the research purpose. For this, I would like to thank you in advance.

Instruction

3. You do not need to write your name or identity.
4. Please provide short answers to each question and by circling the answers for choice questions.

Section I

Questionnaire

Table 2: AA Traffic police officers' reaction to the questions related to awareness-raising activities of AATMA

No	Categories	Yes	No
1.	The Agency works in cooperation with AA Traffic Police to reduce road traffic crashes		
2.	There is good flow of information (communication) between AA Traffic Police and the Agency concerning road traffic crashes.		
3.	The Agency gives awareness-raising (education) on traffic laws and regulations to law enforcing organs like traffic police in organized way		
4.	The Agency organizes capacity building trainings to AA Traffic Police to support the effort of reducing road traffic crashes		
5.	The awareness-raising programs of the Agency played significant role in the reduction of road traffic crashes in the last two years		

6.	Does the Agency work awareness- raising activities in cooperation with AATP?		
7	Are the channels of awareness-raising programs good and effective?		
8	Do the channels require some improvement and update?		
9.	Do you think that the awareness-raising activities of the Agency has good acceptance among road users?		
10.	Do you notice any behavioural change on pedestrians and car drivers as a result of the awareness-raising activities?		

If you have any suggestion about the Agency's awareness-raising activities

What strengths have you noticed so far?

What are the weaknesses?

APPENDIX C

አዲስአበባዩኒቨርሲቲ

የጋዜጠኝነትናኮሙዩኒኬሽንድህረምረቃትምህርትክፍል

ለአዲስአበባትራፊክፖሊስአባላትየቀረበመጠይቅ

የዚህመጠይቅቀናዓላማየአዲስአበባትራፊክማኔጅመንትኤጀንሲበከተማዋበየዕለቱየሚደርሰውንየትራፊክአደጋለመቀነስየሚያካሄደውንየግንዛቤማሳደጊያሥራዎችውጤታማነትለመመርመርመረጃለመሰብሰብነው።

ስለዚህእርሶሚሰጡትእውነተኛመረጃበጥናቱለሚገኘውጤትከፍተኛአስተዋፅኦአንደሚኖረውእሙንነው።

በመሆኑምቀጥለውለሚቀርቡጥያቂዎችእውነተኛመረጃበመስጠትእንዲተባበሩንእየጠየኩእርሶሚሰጡትመረጃ (መልስ) ምስጢራዊናለዚህጥናትአገልግሎትብቻየሚውልመሆኑንከወዲሁእገልጻለሁ።

ለትብብርዎከወዲሁከልብአመሰግናለሁ።

አጠቃላይመመሪያ

ሀ) ስምዎንመጻፍየለብዎትም

ለ) ከታችለቀረቡጥያቂዎችመልስዎንበላጥኖቹውስጥ (✓) ምልክትበማድረግመልሱንይሙሉ።

መመሪያአንድ

1. ግላዊመረጃ

1.1 ጾታ፡- ወንድ ሴት

1.2 እድሜ፡- ከ25 በታች 25-35 36-45 46-55 56 እናከዚያበላይ

1.3 የትምህርትደረጃ፡- ስርተፍኬት ዲፕሎማ ድግሪ ሁለተኛድግሪና ከዛ በላይ

ሌላካለይገለፅ-----

1.4 የሥራልምድ፡- ከ1 ዓመትበታች ከ1-5 ዓመት ከ6- 10 ዓመት ከ11- 15 ዓመት

ከ16- 20 ዓመት ከ21-25 ዓመት 26 ዓመትእናከዛበላይ

መመሪያሁለት

• ከታችለቀረቡጥያቋዎችመልስዎንበሳጥኖቼውስጥ (✓) ምልክትበማድረግመልሶንይሙሉ።

1. የመንገድትሬክአደጋንለመቀነስየአዲስአበባትሬሬክማኔጅመንትኤጀንሲከአዲስአበባትሬሬክፖሊስጋርቦት-ብብርይሠራል

ይሠራል አይሠራምም

2. የመንገድትሬክአደጋንበተመለከተበአዲስአበባትሬሬክፖሊስአናበኤጀንሲውመካከልመልካምየመረጃልውውጥ (ግንኙነት) አለ

አለ የለም

3. ኤጀንሲውበትሬሬክሕጎችናደንበችላይአንድትሬሬክፖሊስላሉየሕግአስከባሪአካላትየግንዛቤማሳደጊያሥልጠናበተደራጀመልኩይሰጣል

ይሠራል አይሠራም

4. የመንገድትሬሬክአደጋንለመቀነስበሚደረገውጥረትውስጥኤጀንሲውለአዲስአበባትሬሬክፖሊስአካላትየአቅምግንባታሥልጠናዎችንያዘጋጃል

ያዘጋጃል አያዘጋጅም

5. የኤጀንሲውየግንዛቤማሳደጊያፕሮግራሞችባለፉትሁለትዓመታትለተስተዋለውየመንገድትሬሬክሞትመቀነስትልቅሚናበራቸው

አዎ አያዘጋጅም

6. ኤጀንሲውከአዲስአበባትሬሬክፖሊስጋርቦመተባበርበመንገድደህንነትላይየግንዛቤማሳደጊያሥራዎችንይሠራል

አዎ አይሠራም

7. የግንዛቤማሳደጊያፕሮግራምመንገዶች (channels)ጥሩናውጤታማናቸውበለውይምታሉ?

አዎ አይደለም

8. የግንዛቤማሳደጊያመንገዶች (channels) ማሻሻያያስፈልጋቸዋል?

አዎ አያስፈልግም

9. የኤጀንሲውየግንዛቤማሳደጊያሥራዎችበመንገድተጠቃሚውዘንድተቀባይነትአለውበለውይምናሉ?

አዎ የለውም

10. የኤጀንሲውየግንዛቤማሳደጊያሥራዎችእግረኞችናአሽከርካሪዎችየባህሪለውጥአንዲያመጡአድርገዋልብለውያስባሉ?

አዎ አላደረጉም

በኤጀንሲውየመንገድደህንነትግንዛቤማሳደጊያሥራዎችላይአስተያየትካሉት

የኤጀንሲውየመንገድደህንነትግንዛቤማሳደጊያሥራዎችንአስመልክቶአንደጠንካራጎንገረጠቅሉትነገርካለ

ደካማጎኖች

APPENDIX D

ADDIS ABABA UNIVERSITY

GRADUATE SCHOOL OF JOURNALISM AND COMMUNICATION

Questionnaire for Pedestrians

Dear respondents,

The purpose of this questionnaire is to collect data in order to investigate the effectiveness of awareness-raising activities to reduce road traffic crash in Addis Ababa. Your genuine and accurate responses will no doubt have great contribution to the findings of the research. Therefore, you are kindly requested to give genuine answers to the questions below. The researcher would like to assure you that the answers that you give will be used only for the research purpose. For this, I would like to thank you in advance.

Instruction

1. You do not need to write your name or identity.
2. Please provide short answers to each question and by circling the answers for choice questions.

Section I

Questionnaire

Table 3: Addis Ababa Pedestrians' reaction to the questions related to awareness-raising activities of AATMA

No.	Categories	Yes	No
1.	Do you have sufficient knowledge about AATMA?		
2.	Do you have sufficient knowledge about some of the duties of the Agency?		
3.	Do you use the print media (brochure, flyers, magazines, newsletters, etc.) of the awareness-raising program?		
4.	Do you use the electronic media (TV, face book, twitter etc.) of the awareness-raising program?		
5.	Do you pay attention to the awareness-raising activities of the Agency?		
6.	Do you think that the awareness-raising activities have positive impacts on pedestrians' road using habit or behaviour?		
7.	Do you think that the Agency is doing its label best to reduce road traffic crashes through its awareness-raising programs?		

APPENDIX E

አዲስ አበባ የኔቨርስቲ

የጋዜጠኝነትና ኮሙኒኬሽን ድህረ ምረቃ ትምህርት ክፍል

ለእግረኞች የቀረበ መጠይቅ

የዚህ መጠይቅ ዋና ዓላማ የአዲስ አበባ ራፊክ ማኔጅመንት ኤጀንሲ በከተማዋ በየዕለቱ የሚደርሰውን የራፊክ አደጋ ለመቀነስ የሚያካሄደውን የግንዛቤ ማሳደጊያ ሥራዎች ውጤታማነት ለመመርመር መረጃ ለመሰብሰብ ነው።

ስለዚህ እርሶ የሚሰጡት እውነተኛ መረጃ በጥናቱ ላይ ገኘው ውጤት ከፍተኛ አስተዋፅኦ እንደሚኖረው እሙን ነው።

በመሆኑም ቀጥለው ለሚቀርቡ ጥያቄዎች እውነተኛ መረጃ በመስጠት እንዲተባበሩን እየጠየኩ እርሶ የሚሰጡት መረጃ (መልስ) ምስጢራዊና ለዚህ ጥናት አገልግሎት ብቻ የሚውል መሆኑን ከወዲሁ አገልግሎት።

ለትብብርዎ ከወዲሁ ከልብ አመሰግናለሁ።

አጠቃላይ መመሪያ

ሀ) ስምዎን መጻፍ የለብዎትም፤

ለ) ከታች ለቀረቡ ጥያቄዎች መልስ ለመስጠት ጥቅም ላይ ያውጡ (✓) ምልክት በማድረግ መልሱን ይሙሉ።

ክፍል አንድ

መመሪያ አንድ

2. ግላዊ መረጃ

1.1 ጾታ፡- ወንድ ሴት

1.2 እድሜ፡- ከ25 በታች 25-35 36-45 46-55 56 እና ከዚያ በላይ

1.3 የትምህርት ደረጃ፡- ስርተፍኬት ዲፕሎማ ድግሪ ሁለተኛ ደረጃ ስኬት ከዛ በላይ

ሌላ ካለዎት ገለፅ-----

1.4 የሥራ ልምድ፡- ከ1 ዓመት በታች ከ1-5 ዓመት ከ6-10 ዓመት ከ11-15 ዓመት

ከ16-20 ዓመት ከ21-25 ዓመት 26 ዓመት እና ከዛ በላይ

ክፍልሁለት

ከታችለቀረቡጥያቄዎችመልስዎንበሳጥኖቼውስጥ (✓) ምልክትበማድረግመልሱንይሙሉ።

1. ኤጀንሲውስለሚያከናውናቸውሥራዎችግንዛቤአሎት?

አለኝ የለኝም

2. ኤጀንሲውየግንዛቤሥራዎችንየሚሠራባቸውየህትመትሚዲያዎች (ብሮሽሮች፣ፍላየሮች፣የዜናመጽሔቶችወዘተ.) ይጠቀማሉ?

አዎ አልጠቀምም

3. የኤጀንሲውንየኤሌክትሮኒክሚዲያ (ቴሌቪዥን፣ፊስቡክ፣ትዊተርወዘተ.) ይጠቀማሉ?

አዎ አልጠቀምም

4. የኤጀንሲውንየግንዛቤማሳደጊያሥራዎችበአፅንኦት (በትኩረት) ይከታተላሉ?

አዎ አልከታተልም

5. የኤጀንሲውንየግንዛቤማሳደጊያሥራዎችአግረኞችየባህሪለውጥበማምጣትየመንገድአጠቃቀማቸውአንዲሻሻልለ ማድረግመልካምአስተዋፅኦአድርገዋል?

አድርገዋል አላደረጉም

6. ኤጀንሲውበግንዛቤማሳደጊያሥራዎቹየተቻለውንሁሉበማድረግየመንገድትራፊክአደጋአንዲቀንስአድርጓል

አድርጓል አላደረገም

APPENDIX F

ADDIS ABABA UNIVERSITY

GRADUATE SCHOOL OF JOURNALISM AND COMMUNICATION

Interview Guide Questions for Heads of the Agency

1. In the effort to reduce road traffic crashes, the Agency undertakes awareness-raising activities in Addis Ababa.
 - a) What are the modes of communication?
 - b) So far what are the strengths of awareness-raising activities?
 - c) The weaknesses noticed?
 - d) What are the major challenges of the awareness-raising?
2. What action do you think is taken to increase the participation of stakeholders in the awareness-raising program?
3. What are the tools of awareness-raising activities done by the Agency?
4. What other activities does the Agency do to reduce road traffic crash?
5. What additional efforts need to be done to enhance the awareness-raising activities of the Agency?

APPENDIX G

አዲስ አበባ ዩኒቨርሲቲ

የጋዜጠኝነትና ኮሙኒኬሽን ድህረ ምረቃ ትምህርት ክፍል

ለመንገድ ደህንነት ትምህርት እና አቅም ግንባታ ዳይሬክቶሬት አመራሮች የተዘጋጀ ቃለ መጠይቅ

1. የመንገድ ትራፊክ አደጋን ለመቀነስ ኤጀንሲው በአዲስ አበባ ከሌሎች ሥራዎች በተጨማሪ የግንዛቤ ማሳደጊያ ሥራዎችን ይሠራል። በዚህ መሠረት
 - ሀ) እነዚህን ተግባራት ለማከናወን የሚጠቀሙ ስልጠናዎች (modes of communication) ምን ድርጅቶች ናቸው?
 - ለ) እስካሁን ባለው ጊዜ የኤጀንሲው የግንዛቤ ማሳደጊያ ጠንካራ ጎኖች ምን ድርጅቶች ናቸው?
 - መ) የተስተዋሉ ደካማ ጎኖች ስ?
 - ሠ) ዋና ዋና ተግዳሮቶቹን ለገልጹልን?
2. በግንዛቤ ማሳደጊያ ሥራዎች ስጥ የባለድርሻ አካላትን ተሳትፎ ለማሳደግ የተሠሩ ሥራዎችን ለገልጹልን?
3. ኤጀንሲው የግንዛቤ ማሳደጊያ ሥራዎችን የሚሠራባቸው መንገዶች/ ዘዴዎች/ ምን ድርጅቶች ናቸው?
4. የግንዛቤ ሥራዎችን ጤታማነት ለማሻሻል /ለማሳደግ/ ኤጀንሲው በተጨማሪ ሠራባቸው የሚገቡ ምን ድርጅቶች ናቸው በለው ይገምታሉ?
5. በእርስዎ አመለካከት የመንገድ ትራፊክ አደጋን ለመቀነስ ኤጀንሲው ሌላ ሠራቶች የሚገቡ ሥራዎች አሉ የሚሏቸው ካሉ ለገልጹልን?

APPENDIX H

ADDIS ABABA UNIVERSITY

GRADUATE SCHOOL OF JOURNALISM AND COMMUNICATION

Guide Questions Presented for Focus Group Discussion Participants from AATMA

Dear participants,

The purpose of this focus group discussion is to open discussion for a research being done on investigation of the effectiveness of awareness-raising activities to reduce road traffic crashes in Addis Ababa. So, your genuine answer and explanation plays a key role in the findings of the research. I would like to thank you for having you as participants.

1. Some findings explain that the basic cause of traffic injuries is due to human error which can be in fact preventable. Do you think that awareness-raising activities play key role to prevent road traffic crashes? How?
2. Do you think that the Agency is giving more emphasis/attention to road safety education/ awareness-raising activities than enforcement and engineering? Why?
3. To which aspect of road safety does the Agency give prioritization (awareness-raising programs, enforcement or engineering)? Why so?
4. In the last two years, road traffic fatalities tend to decrease. What do you think is the main factor for this? Has the awareness-raising program played significant role for the reduction?

APPENDIX I

ADDIS ABABA UNIVERSITY

GRADUATE SCHOOL OF JOURNALISM AND COMMUNICATION

Guide Questions Presented for Focus Group Discussion Participants from AATP

Dear participants,

The purpose of this focus group discussion is to open discussion for a research being done on investigation of the effectiveness of awareness-raising activities to reduce road traffic crash in Addis Ababa. So, your genuine answer and explanation plays a key role in the findings of the research. I would like to thank you for having you as participants.

1. AATMA is a responsible organ for the three road safety aspects: awareness creation, enforcement and engineering in Addis Ababa, how do you describe the role of the awareness program of the Agency in the reduction of road traffic fatalities?
2. How can road traffic crashes be reduced?
3. Statistical evidences show that for the last two years fatal injuries due to road traffic crashes have gone down to some extent. What do you think is the main cause? How do you explain the contribution of the awareness-raising program?

APPENDIX J

አዲስአበባዩኒቨርሲቲ የጋዜጠኝነትና ኮሙዩኒኬሽን ድህረ ምረቃ ትምህርት ክፍል

ለአዲስአበባትራፊክፖሊስአባላትየቀረበዮቡድንውይይት

ክቡራንተሳታፊዎች፣

የዚህየቡድንውይይትዓላማበከተማችንአዲስአበባየመንገድትራፊክአደጋንለመቀነስበአዲስአበባከተማአስተዳደርትራፊክማኔጅመንትኤጀንሲየሚሠሩየግንዛቤማሳደጊያሥራዎችውጤታማነትንለማጥናትውይይት ማድረግሲሆንለጥናቱውጤታማነትየእርሶትክክለኛመረጃበጥናቱለሚገኙግኝቶችወሳኝነው።

በጥናቱዘሪያሃሳብንለመስጠትተሳታፊለመሆንጊዜዎንበመስዋትተሳታፊበመሆኖአመሰግናለሁ።

1. እንደሚታወቀውየአዲስአበባትራፊክማኔጅመንትኤጀንሲበአዲስአበባከተማውስጥበሦስቱየመንገድደህንነትሥራዎችማለትምበግንዛቤ፣በደንበወይስበኢንጂነሪንግሥራዎችሕጋዊኃላፊነትአለበት። ለመንገድትራፊክአደጋመቀነስየኤጀንሲውንየግንዛቤማሳደጊያሥራዎችንሚናእንዴትይገልጹቻቸዋል?
2. በእርስዎእይታየመንገድትራፊክአደጋእንዴትሊቀነስይችላሉ?
3. የስታቲስቲክስመረጃዎችእንደሚያሳዩትላለፉትሁለትዓመታትበመንገድትራፊክምክንያትየሚከሰተው የሞትአደጋእየቀነሰመጥቷል። ለዚህዋናውምክንያትነውየሚሉትምንድነው? ለዚህምየግንዛቤማሳደጊያሥራዎችአስተዋፅኦእንዴትይገለጽ?

APPENDIX K

Summary Report on Road Traffic Deaths

ADDIS ABABA

Summary Report on Road Traffic Deaths



This report presents summary findings on fatally injured victims from road traffic crashes in 2019/20 in Addis Ababa.

Trend in road traffic deaths, 2012/13 - 2019/20



Road traffic deaths dropped in 2019/20 after continuous increases since 2012/13.

2012/13 2013/14 2014/15 2015/16 2016/17 2017/18 2018/19 2019/20

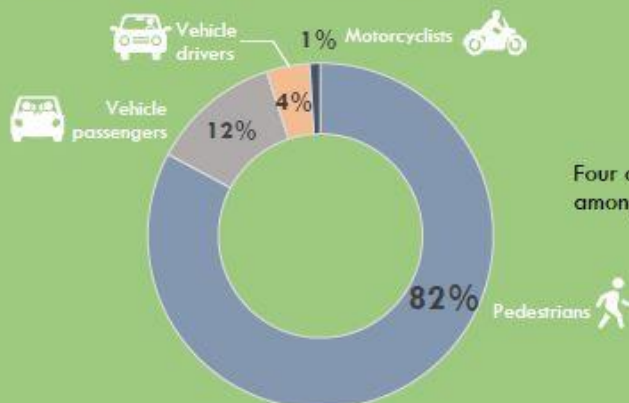
Deaths per 100,000 population, 2013/14 - 2019/20



Deaths per 100,000 population dropped in 2019/20 compared to the previous years.

2013/14 2014/15 2015/16 2016/17 2017/18 2018/19 2019/20

Deaths by road user type, 2019/20



Four out of five road traffic deaths were among pedestrians.



Deputy Director General of AATMA, Eng. BirhanuGirma



Director of Road Safety Education and Capacity Building Directorate, Genet Dibaba



Ato Hagerie Hailu AATMA Road Safety Capacity Building Team Leader



FGD Participants at AATMA